



BOONE COUNTY, MISSOURI

Request for Proposal #: 19-01MAY12 – Landscaping Services for Courthouse Plaza Renovation

ADDENDUM #1 - Issued April 9, 2012

This addendum is issued in accordance with the Introduction and General Information of the proposal and is hereby incorporated into and made a part of the Request for Proposal Documents. Offerors are reminded that receipt of this addendum should be acknowledged and submitted with Offeror's proposal response.

Specifications for the above noted Request for Proposal and the work covered thereby are herein modified as follows, and except as set forth herein, otherwise remain unchanged and in full force and effect:

- 1) On April 4, 2012, the United States District Court for the Eastern District of Missouri entered an injunction prohibiting the enforcement of Missouri's Excessive Unemployment Law, Sections 290.550-290.580. In light of that injunction, please delete page 49 of the RFP – *Excessive Unemployment and Restrictive States*.
- 2) The County has made available for information purpose, copies of the Courthouse Plaza Plan from when it was originally built. Offeror's may pick-up this plan in the Purchasing Office, Room 110, 613 E. Ash, Columbia, MO 65201 between the hours of 8:00 a.m. – 5:00 p.m, Monday - Friday.
- 3) Attached is the *Avenue of the Columns Master Plan* for information purpose.
- 4) Paragraph under 3.3 – Scope of work reads:

Offeror shall itemize schematic(s) with associated costs. County reserves the right to purchase design elements from multiple Offerors, so all Offerors must clearly outline any cost to County for purchasing select design elements even if Offeror is not the awarded the entire project contemplated by Offeror. County will only purchase individual elements from responsive proposals that have bid the entire project.

The following clarification is provided to this paragraph:

The County plans to negotiate and award to one Offeror from this Request for Proposal. However, there may be elements of design in another Offeror's proposal response that the County may want to incorporate into the awarded Offeror's plan. Therefore, Offerors should provide a cost for the County to purchase their schematic design in its entirety so the County can pick and choose from those design elements.

A final plan will be approved by the County prior to the Notice to Proceed being issued to the Contractor.

- 5) Grass pave shall be on each side of the sidewalk in the plaza area. Total width should be 20' which includes grass pave and sidewalk. Note: Walnut Street entrance to plaza and sidewalks other than

plaza shall not have grass pave. If there are sidewalk areas wide enough for a truck then grass pave is required. If the sidewalk is narrowed to not handle a vehicle, no grass pave is required.

- 6) Attached are pictures of stainless steel artwork that the County has in storage. The artwork was originally used in the river garden when it was operational. This artwork may be incorporated into Offeror's conceptual design plan. There are 10 sheets of artwork, 140" long x 37" wide, and the legs are 12".
- 7) Add to paragraph 3.3.A. - Submit a conceptual design plan from several different angles.
- 8) Add to paragraph 3.3. A. - 19. At the close of the project, the Contractor shall provide the County with a list of recommendations for the care of the perennial plants.

By: 
Melinda Bobbitt, CPPB
Director of Purchasing

OFFEROR has examined copy of Addendum #1 to Request for Proposal # 19-01MAY12 – Landscaping Services for Courthouse Plaza Renovation receipt of which is hereby acknowledged:

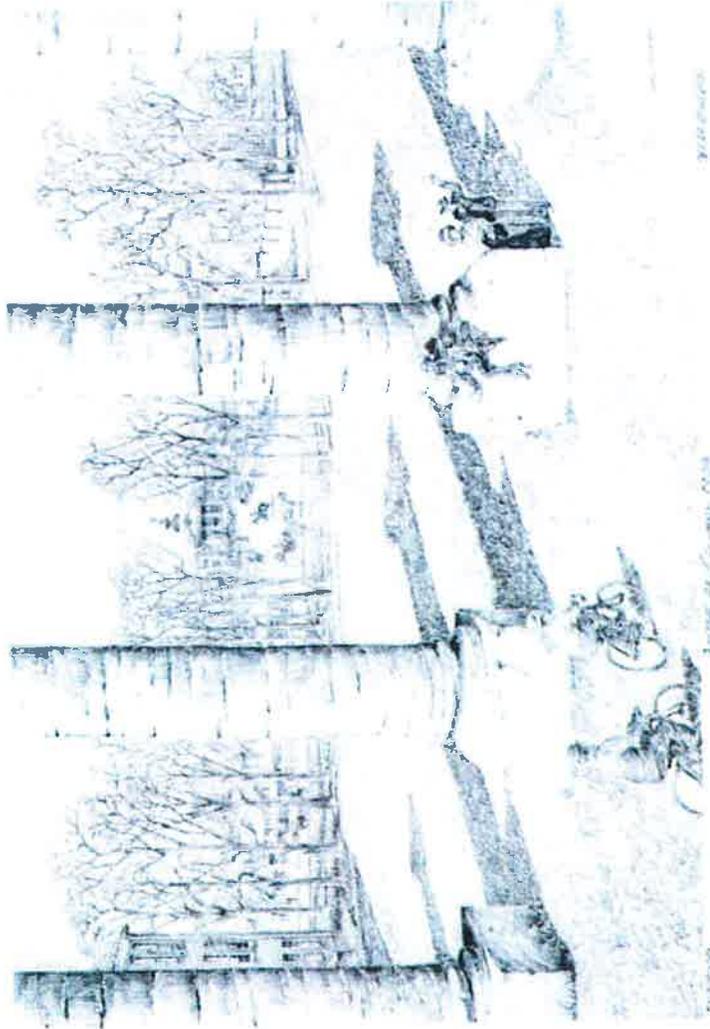
Company Name: _____
Address: _____

Phone Number: _____ Fax Number: _____
E-mail address: _____

Authorized Representative Signature: _____ Date: _____

Authorized Representative Printed Name: _____

AVENUE OF THE COLUMNS
COLUMBIA, MISSOURI



MASTER PLAN



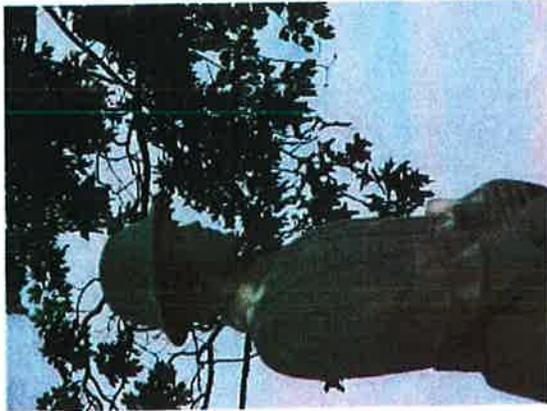
Prepared by:
Jeffrey L. Bruce & Co.
North Kansas City, Missouri

April 11, 2005
Final Draft



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Introduction

The Eighth Street Corridor is central to the history and growth of Columbia since its founding in the early 1800s. Linking the historic columns at the courthouse square to the columns cast in the Francis Quadrangle foreground of Jesse Hall, the Avenue of the Columns serves to remind us of the important relationship between Columbia and the University of Missouri. This legacy casts a unique and exciting character on this corridor, and focuses the potential for realizing a vision of vigor and excitement in the future of the Avenue of the Columns and Downtown Columbia as a whole.

The Goal of this Master Plan is to provide recommendations for re-characterizing and invigorating the Avenue of the Columns including implementation strategies focused on capturing the potential of the corridor and rephrasing the corridor from its "9-5" character and realize the potential and opportunities that exist.

Many challenges confront this endeavor, and success will ultimately depend on forward thinking creative efforts through design and implementation at all levels from the regulatory framework to the development community.

Recommendations of this plan build on the strengths of the exiting neighborhood and business framework to provide for improvements focused to improving the interaction of the pedestrian in the urban framework. This plan also identifies opportunities to re-characterize the urban context including planned and potential civic, retail, office, and residential uses.

Project Description

Jeffrey L. Bruce & Company (JBC) was selected in the Fall of 2001 to participate in a design competition for the Master Plan of a Streetscape for the Avenue of the Columns. Proposing a concept that envisioned a result extending beyond the streetscape to the fabric of the downtown, JBC was selected and ultimately authorized in the Summer of 2003 to initiate a planning process to study and develop short and long term strategies for the revitalization of the Avenue of the Columns.

Organization of this Plan

The Avenue of the Columns Master Plan is presented in five parts.

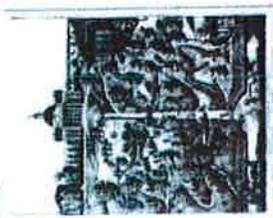
- The first part consists of a background section, establishing a basis for the project through discussions of the community, evolution of the project and acknowledgment of the vision supporting this effort.
- Part two discusses the framework within which the project is perceived, describing the project area, its physical context and the functional framework that the master plan process responds to.
- The third part of this master plan discusses our approach to enhancing the pedestrian environment by presenting functional tools that are available to create successful streetscapes and related opportunities. This section breaks down the streetscape into functional zones and gives meaning and purpose to the elements that are considered in each.
- The Streetscape Master Plan is discussed in detail as part four, looking at recommendations for the proposed streetscape footprint within each block, key focus areas within the Avenue, and recommendations for streetscape elements that will be key components of the streetscape vernacular.

- Part five is a discussion of implementation issues and recommendations on matters such as phasing, development tools, etc.
- Within the appendix, we have documented the committee driven process providing insight into the community aspect of the master planning process. Also, attached to the appendix is a general glossary of streetscape terminology for reference and an opinion of implementation costs for future use.





The courthouse sits on the site of the Boone County Courthouse, which was built in 1846. It is the only building in Boone County that has survived the ravages of time. At this site, the Boone County Courthouse stands as a testament to the history of Boone County.



History

Columbia's history has been significant and meaningful both as a prominent community in the great American west and as a leading University town west of the Mississippi. The Avenue of the Columns captures many aspects of this history as reminders of the unique history and importance of Columbia.

Many native tribes roamed this area of the west with the Osage and Missouri Indians in particular frequenting many parts of Missouri in the times prior to Lewis and Clark's expedition which took place in the early 1800s.

Originally a settlement named Smithton, the establishment moved east across the Flat Branch due to a lack of water and was renamed Columbia in 1821. The City of Columbia was formally organized in 1892 and became incorporated in 1949.

The University of Missouri was founded in 1839, the first State University west of the Mississippi River. On January 9, 1892 the school's main academic building, simply called Academic Hall, burned to the ground, leaving little more than six stone columns standing. The columns built from local limestone still stand today at the south terminus of the Avenue of the Columns and have become a symbol of the campus and form the center of Francis Quadrangle, the oldest part of campus.

The Courthouse Square, anchoring the northern end of the Historic Avenue of the Columns, has become the jewel of downtown. Flanked by the

Roger B. Wilson Boone County Government Center and the Boone County Courthouse, the square has become the premier gathering place for the community.

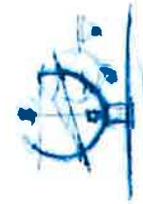
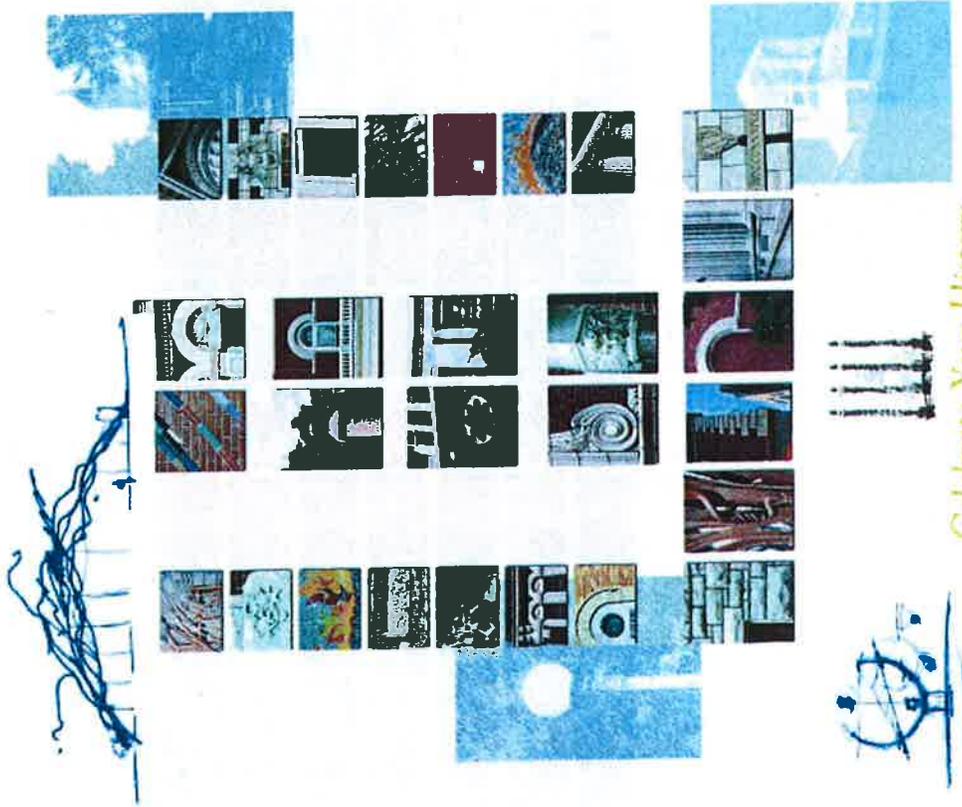
Boone County used the original courthouse located on this site throughout the 19th century. When the 1906-09 courthouse neared completion, the County Court ordered the old building razed. After a concerted effort by townspeople to save the building, the decision was made to preserve the columns. Today, the columns of the 1846 courthouse and the columns of the University's Academic Hall stand in their original alignment of the mid-19th century providing iconic symbols of the remarkable town / gown history of Columbia and speak to the significance of the Avenue of the Columns.

Vision

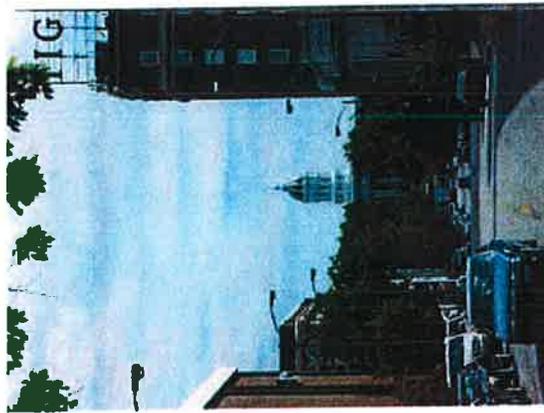
Guiding this effort is a vision for the Avenue of the Columns that began to take shape in the early 1990s, seeking to capture the promise and potential of downtown redevelopment in a time when the model for most communities was a type of "secular suburbanism". At that time a group of community leaders pursued a vision for the Avenue of the Columns and downtown that would return the focus to community potential through identity and social interaction; turning resources inward and drawing upon the history and strengths that make Columbia unique and exciting. As time passed, the downtown did begin to revitalize and the unique potential of the Avenue of the Columns became even truer.

The vision that began in the early '90s is clearer and closer today with many of the ideas from that time carrying forward to this process. The vision statement guiding this effort is as follows:

"The mission of the Avenue of the Columns Committee is to create a beautiful, historic walkway from Columns to Columns that incorporates landscaping, trees, brickwork / stonework, and creative lighting that encourages the development of retail and gathering space wherever possible. The effort should focus on the history of Columbia, especially as it relates to the Town / Gown relationship."



Celebrate Your History



Avenue of the Columns Design Objectives

Responding to this vision this Master Plan pursues these specific objectives:

- *Guide the nature of Public & private improvements*
- *Create a pedestrian oriented environment that is safe, visually pleasing, accessible, and comfortable.*
- *Promote sidewalk interest and activity support commercial interests*
- *Enhance the character of downtown*
- *Reinforce the unique character of Columbia*
- *Integrate diverse uses into a common fabric*
- *Maintain a sense of connection to the history of the community*
- *Unify the visual image of the Avenue of the Columns by implementing a series of streetscape improvements*

planning process and help to envision a resolution to the many complex issues that result from such an undertaking.

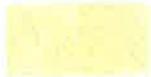
The committee participated in numerous exercises and input sessions focused toward realizing the truths about the Avenue of the Columns and the surrounding community. Their input provided insight and validation of elements of identity and concepts of community forming the basis for an approach that is specific to the project area and focused on the unique qualities of the Avenue of the Columns, Columbia and the University. Throughout the process the committee helped to shape the major vision themes and established goals and objectives that would determine the success for the project. As issues and ideas were formulated, they were tested against the diverse perspective of the committee to ensure broad support of the recommended initiatives and strategies of the plan.

Process Summary

The Master Plan process was designed to understand the range of opportunities and challenges facing this revitalization effort and to develop trust and advocacy through the community and University that will aid in the implementation process through each phase.

Guiding the Master Plan process was a committee comprised of community leaders whose insight and leadership shaped the outcomes of the

Additional community and University input was sought through focused meetings with the University planners, private developers, and community leaders; and public input was collected at the fall twilight festival.



The Avenue of the Columns Committee was comprised of the following individuals:

ROSTER

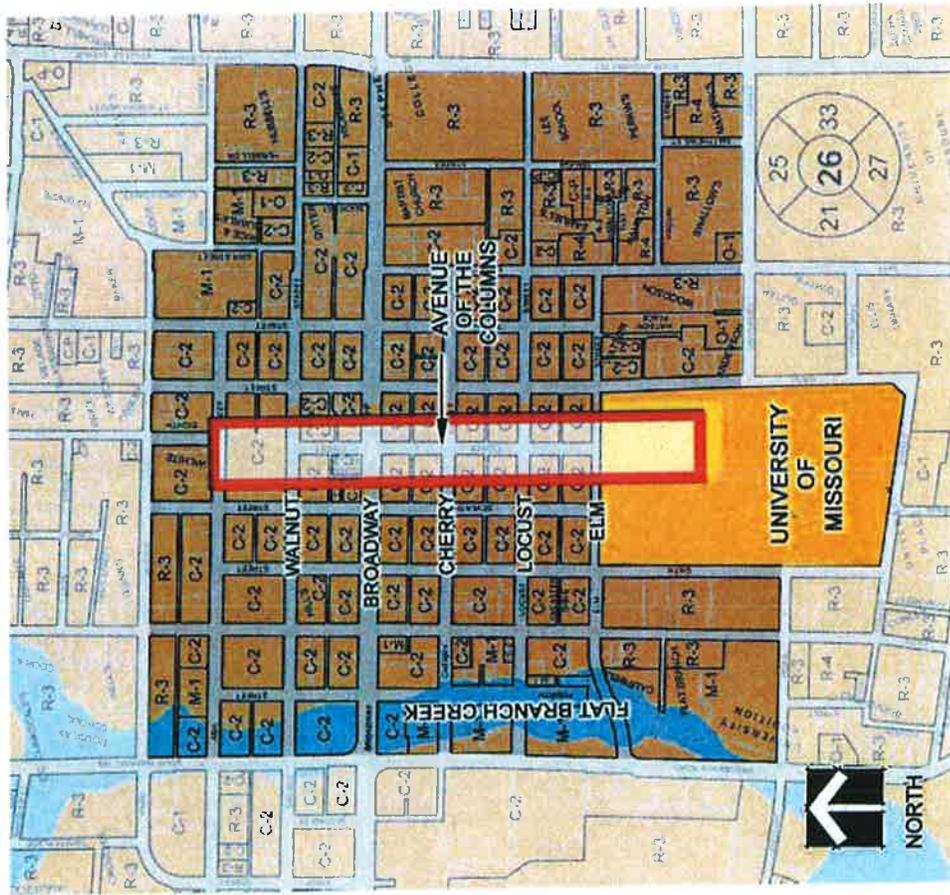
- | | |
|----------------------------------|---|
| Mary Wilkerson – Committee Chair | Boone County National Bank |
| Ray Beck | City of Columbia |
| Jeff Brinegar | Bank of America |
| Doug Crews | Missouri Press Association |
| Charlie Digges, Jr. | The Insurance Group |
| Arnie Fagan | Cool Stuff |
| Bo Fraser | Boone County National Bank |
| Carrie Gartner | Downtown Columbia Association |
| Libby Gill | Special Place Development |
| Randy Gray | Boone Hospital Foundation |
| Kee Groshong | Tofle, Oxenhandler & Hajicek, Attorneys |
| Jan Grossman | City of Columbia |
| Brian Hajicek | City of Columbia Office of Cultural Affairs |
| Martha Hills | Columbia College |
| Darwin Hindman | University of Missouri-Columbia |
| Marie Nau Hunter | D & M Sound Building |
| Bob Hutton | First National Bank |
| Mark Jarvis | Harry S Truman Veterans Hospital |
| Sherman Kelly | Tiger Hotel Building |
| Karl Kruse | John Phillips Architects |
| Sabrina McDonnell | NCCNA |
| Hew McElroy | Boone County Commission |
| John Ott | Simon Oswald Associates |
| Linda Phillips | Missouri School of Journalism |
| Linda Rootes | Vangel Associates |
| Keith Schnarre | Calvary Episcopal Church |
| Shelly Simon | Walther Antel & Stamper PC |
| Jim Sterling | City of Columbia |
| Mike Vangel | |
| Remy Wagner | |
| Skip Walther | |
| Bill Watkins | |



Background



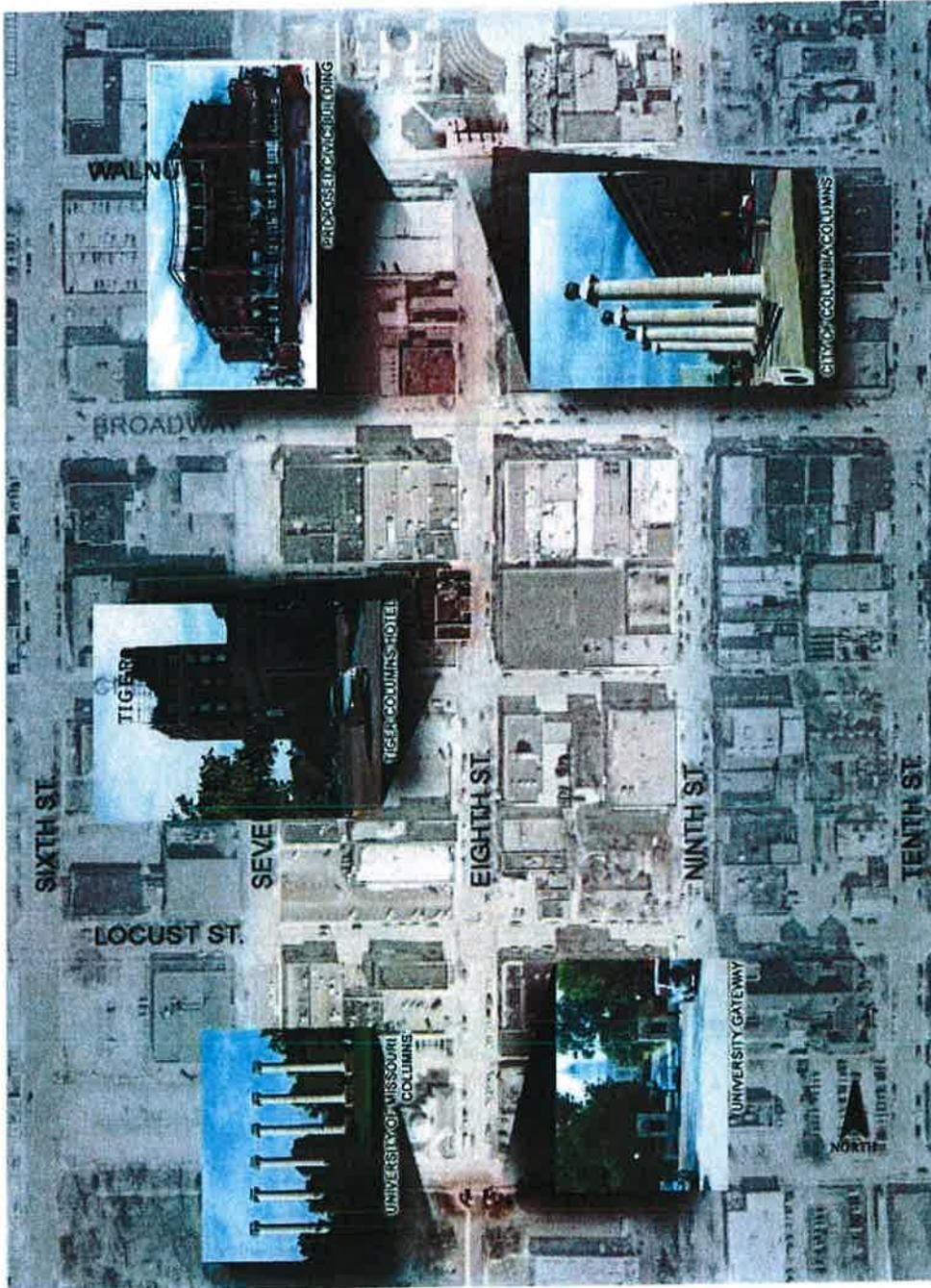
Study Area



Framework



Street Character



Framework



Preliminary Hard-Soft Uses Analysis
Framework

Streetscape Framework

The overall objective of this plan is to establish a unifying streetscape character framework for the Avenue of the Columns that is expressed in a base palette of dimension, elements, materials and finishes.

is the logical next step in implementing the recommended improvements. Specific design of these improvements can be completed by city staff or outside consultants; and can take the form of either traditional design and construction documents, or a streetscape design manual.

As the process evolved there were several areas within the Avenue of the Columns that were uniquely characterized by the functional and streetscape character they exhibit. These functional districts and typologies of use along the avenue begin to demonstrate the reasons for the nature of the streetscape today and can serve as the basis for change in future efforts. Coupled with the analysis of hard and soft uses, areas of commonality of use and perspective can be realized as complimentary to the future of the avenue or as having potential for change within the framework of these efforts.

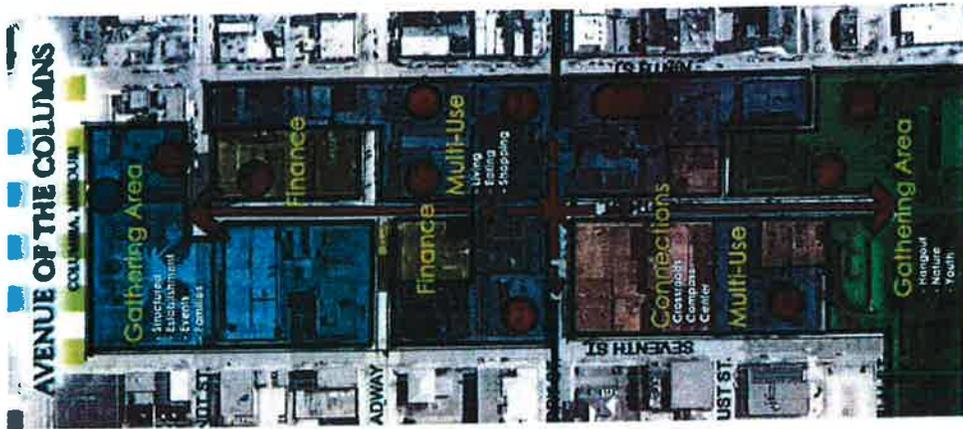
These characterizations become the basis for evaluating individual segments of the corridor and serve to capture the potential for their inclusion in the evolving streetscape. While it is assumed that the identity of the entire avenue will be unified functionally and aesthetically by common treatments in dimension and material, unique aspects of the corridor should also be reflected in the streetscape whether functional or material in nature.

While this plan provides the framework for realizing and acknowledging the strategies and components of the streetscape, specific design of the individual component aspects of the streetscape are beyond the scope of this Plan, and

An interesting and useful way of viewing the Avenue of the Columns is through the characterization of functional groupings of buildings or districts that are shaped by adjacent uses and pedestrian use. The diversity of these districts speaks to the different roles that the streetscape assumes in supporting these functions and the challenges of finding unifying themes and materials that will provide signature and identity to the entire corridor.

Not always distinct in that some districts may overlap others, functional districts are characterized by similarities of use and are often similar in their architectural fabric through scale, style, and detail. Generally anchored by one or two buildings of significance, functional districts respond to these keynote functions either by supporting or servicing the people that use them and work there.

Functional districts that exist within the Avenue of the Columns are as follows:



Preliminary Districting Analysis



Preliminary Concept Development

Framework



Civic & Government

Beginning at the courthouse square and embracing the west side of 8th street to Broadway, government uses and supporting services prevail in this area. The future location of a new municipal complex and City Hall will anchor this district at the northwest corner at Broadway. The architectural character is generally institutional in nature, acknowledging the historical character of the area through materials and reflective detailing.

Generally shaped by a traditional business model, the hours of use in this area is typically 8 to 5, generating a good deal of pedestrian scale traffic and vehicular movement on the street. The courthouse square serves as a community wide stage for gathering and celebration, hosting many events of significance and celebration, focusing the meaning of this area in the downtown fabric.



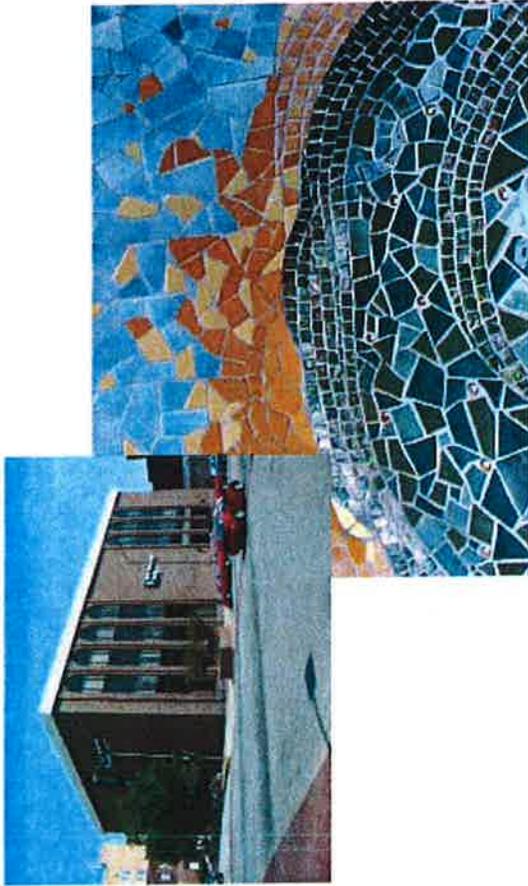
Framework

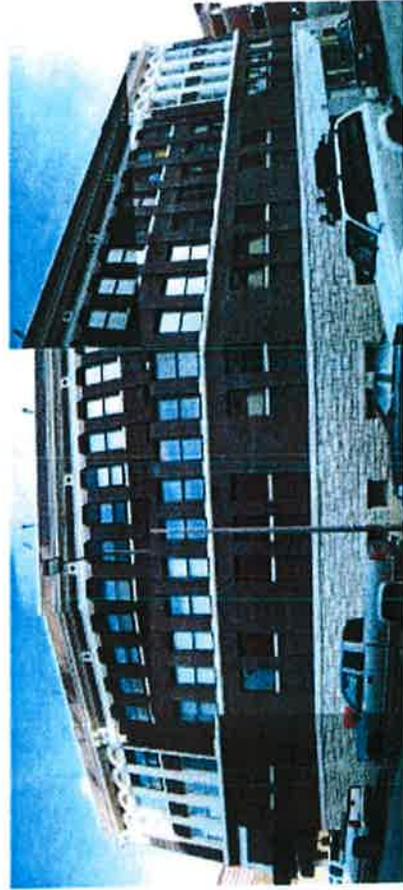
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Institutional

Capturing primarily the northeast and southwest corners of the intersection of Broadway and 8th Street, prominent community banking interests characterize the function of the street through the nature of their user groups and the way they interact at the street level.

Again the pedestrian use of the streetscape in these areas is by customer and employee, traditionally 8 to 5, and generating a good amount of activity in the streetscape and in vehicular movement. Both of these institutions support community art and have prominent installations at the street level.





Framework

Historic Mixed Use

There are many areas of the Avenue of the Columns that are mixed use in character; generally described as a combination of retail and residential. Mainly found to the north end of the Avenue, the district consists of buildings occupying the east side of the corridor from Walnut south to Broadway and both sides of the street from Broadway to the parking garage north of Cherry. These uses generate diversity of activity due mainly to the range of hours that users occupy or move within the streetscape.

An important and signature component of this district is the Tiger Columns Hotel, which has a long and storied legacy in the community. The Tiger Columns serves as a unique residential community in the urban fabric, generating nontypical pedestrian use of the streetscape and a need for special consideration of issues such as accessibility, diversity of goods and services, and a breadth of consideration and style when evaluation design, art and streetscape amenities. Mixed uses are critical to the vitality of the street and breadth of the functional character, and should be acknowledged in streetscape improvements through dimension, focus, and other forms of enhancement.

Redevelopment

Beginning south of Cherry and running to mid-block between Locust and Elm, particularly on the west side of 8th Street, the streetscape character becomes diluted with open space and underutilized buildings of mixed function and architecture. While there are some active enterprises in this district, the general conclusion of the process is that its ultimate potential lies in redevelopment to a mixed use block involving combined retail and residential uses in a traditional neighborhood architectural footprint.

This redevelopment potential is critical for the entire Avenue of the Columns as it would fill in a significant functional gap in the procession of movement and activity along the corridor. The void of activity and streetscape character (environment, intimacy, identity) currently separates the Avenue of the Columns into north and south portions, polarizing the corridor and disrupting the continuity of movement and progression of memory in such a way that it disrupts the entire sense of place. Closing this gap with a vibrant “dawn to dark” character that is diverse in use and function, would serve to animate this district and inject vitality into the entire Avenue of the Columns.

An important aspect of the redevelopment area would be the creation of architectural elements and space that reflect the sense of history and community scale that is evocative of Columbia’s past and consistent with neighborhood architecture. Street level interaction, pedestrian scale detail, and

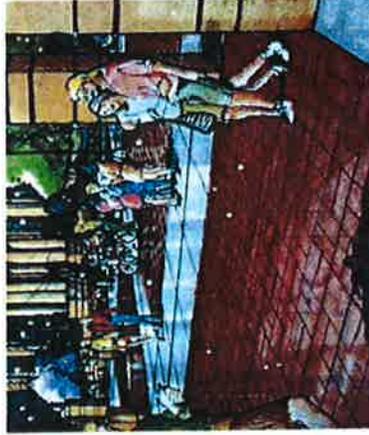
interactive residential design are important to the support they offer street level activity and vibrancy.

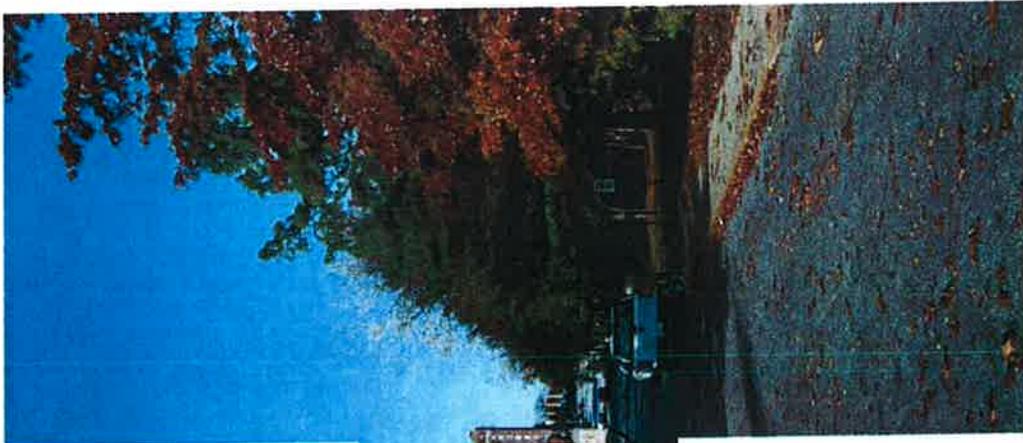
General guidelines for restoration and infill within the development zone:

- *Building façades and fronts should be oriented to the street for relevancy to activity within the streetscape and to provide definition to the streetscape space.*
- *Ground floors of building frontages should be at the or near the right of way boundary and should consist primarily of retail uses.*
- *Buildings should be designed to reflect the history and character of downtown Columbia and should be consistent in scale and proportion to surrounding structures.*
- *Building heights should not exceed 3-4 stories and should focus on shaping human scale and detail at street level. Large undifferentiated areas of façade should be avoided by creating relief in the façade plan through recessed windows and doors, projecting architectural elements, focal elements / scale at entries, and integrated art or ornament.*

The ground level or base components of the building should be transparent in character to encourage pedestrian interaction and exchange at the street level.

Building lighting should accentuate important architectural features such as entries, towers, cornices, and repetitive columns or bays.





University

Consistent with its historical legacy, University activity and function occupies a great deal of the 8th Street corridor at its southern end. While the traditional north boundary of the University lies at Elm Street, the functional boundary creeps further into the Avenue of the Columns, extending to mid-block between Locust and Elm on the west and to Locust on the east. Focused on the Journalism school, the University District is a vibrant “24 hour” streetscape drawing energy and vigor from the student population it serves.

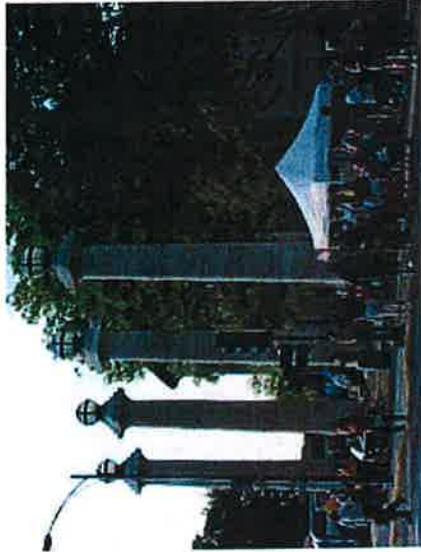
Extending through the campus gateway at Elm to the Columns in Francis Quadrangle, the history and meaning of this district is vital to the identity of the Avenue of the Columns and its inclusion and consideration critical to the identity of the corridor.

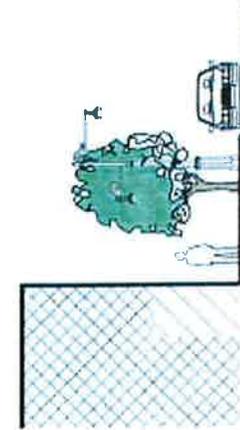
Enhancing the Urban Environment

Qualities of "Pedestrian Friendly Streets"

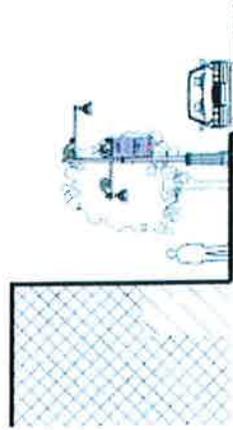
Successful street environments take many forms. Some distinguished by their commercial bustle, others by wide sidewalks or quality of surrounding architecture. Regardless of their shape and size, most good streets obtain their "friendliness" from three conditions:

- Safe and comfortable *environment*
- A sense of human scale or *intimacy*
- Distinctive character, or a sense of *identity*





Street Trees



Signage

Environment

“Environment” means the basic conditions by which a street is initially and fundamentally perceived as comfortable and approachable (or conversely, as uninviting or threatening). Streetscape elements that contribute to a feeling of comfort and approachability are street trees (shade), clear and accessible wayfinding systems, and barrier free pedestrian ways.

- Inclusive wayfinding should guide people throughout the corridor to points of interest, use, and movement. These systems could be coordinated with other mapping or visual cueing systems, providing a unified downtown aesthetic to materials of communication.

- Sidewalks should be bumped out at intersections and key mid-block points to create dimension and space for social interaction and engagement. These areas can be keyed with planters and benches or could serve as locations for informational installations or civic art.

Along the Avenue of the Columns, a sense of comfort and approachability can be achieved through modifications of the streetscape and would enhance the general perception of the corridor.

Guidelines

- Street trees should be placed at curbside on all streets, evenly spaced at 20' to 35' depending on infrastructure constraints and site limits.
- ADA standard accessible ramps should be placed at all crosswalk and drive cus.
- Pedestrian scale street lighting should be oriented and focused on the sidewalk and pedestrian social spaces.

Intimacy

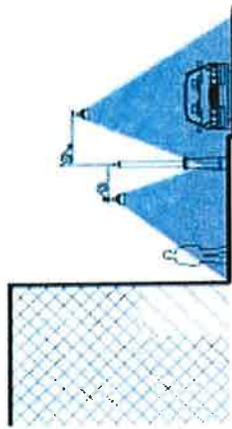
“Intimacy” relates to the scale and focus of streetscape elements supporting and serving pedestrian movement and interaction. Contrasted to those elements of the streetscape that are vehicle oriented, the pedestrian environment is the sidewalk where intimacy is realized within the bubble of activity between the architectural framework and the street. Following this concept, there should be clear delineators between functional types of activity such as barriers or distinction between sidewalks and traffic where they abut.

An important component of this streetscape enclosure is the presence of architectural structure at one edge, generally at the right of way line. Where this structure does not exist, such as when sidewalks abut open parking, the structure should be implied by vertical plantings or other type of implicit screening. Lighting provides additional shape to this structural perception, so that where there is street lighting, supplementary sidewalk-scale lighting is desirable. Street furnishings or multifunctional features such as raised planter seatwalls or artwork forms also contribute to the intimacy of sidewalk spaces for pedestrians.

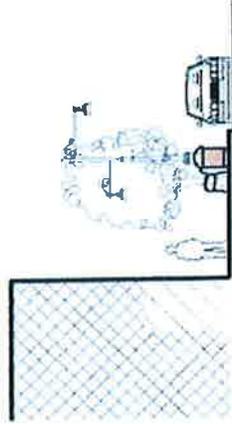
Clarity of pedestrian movement is important so that people know where to gather, how to move through spaces, and where areas of importance or focus are located. This can be achieved through patterning of the ground plane in paving patterns, textures, colors and paving treatments.

Guidelines

- Functional definition and form should be developed at the streetscape within the pedestrian zone differential paving pattern.
- Paving patterns, colors, and types should clearly define areas of travel, gathering and importance along the streetscape.
- Continuous banding should be developed at curbside to further define the pedestrian sidewalk zone. This band can also serve to expand the planting domain of street trees.
- Sidewalk focus areas and crosswalks should be enhanced with special paving treatments and incorporate accessible ramps and signals.
- Pedestrian scale lighting should be implemented either as a component of street lighting or as stand alone fixture that complement street lighting.
- Vertical plantings or ornamental open fencing should be placed where sidewalks abut open parcels or parking provide for edge definition to the pedestrian zone.



Safety and Pedestrian Lighting



Street Furnishings

- Street furnishings and others social amenities should be provided to facilitate pedestrian scale interaction within the pedestrian zone. Location of these components should respond to areas of focused activity, nodes of interaction or points of focus along the Avenue.





Identity

“Identity” is attained through meaningful or distinctive streetscape character, realized through the use of unique or creative elements that add life or significance to the streetscape experience. Elements that associate with historical, artistic, or cultural meaning provide distinction and focus to the area and an opportunity to “keynote” areas of heightened social activity, commercial interaction, or community space.

Through the use of elements such as building awnings, banners, planters, colorful paving, and civic art, the pedestrian can better understand “where the action is” and affirm the commercial and civic vitality of the Avenue corridor.

Guideline:

- *Street level structures should be encouraged to place colorful awnings and interesting signage on their fronts. Storefronts should be as transparent as possible, allowing free visual exchange between the street activity and the in-store activity. This can be especially valuable at night as light from inside the establishment will play onto the adjoining walk and animate the streetscape.*
- *Each intersection should be signified by a vertical architectural element that effects a transition from the pedestrian scale interaction at street level and the scale of the surrounding architecture. These could be artistic and symbolic in nature but should be architectural in material and character.*

- *Raised planters and seating areas should be located at each intersection and node of activity to highlight these areas and foster social gathering and interaction.*
- *Banners can be integrated into light poles to provide for event identifiers or signifiers of important identity elements.*
- *Civic art should be integrated into the planter walls (medallions or signage), paving (inserts), and other streetscape elements. Site specific art installations, either permanent or revolving, should be located at each intersection or node of activity creating a consistent movement of art in the streetscape and reinforcing a progression of memory from space to space throughout the entire corridor.*
- *Light pole bases and bollards should be fashioned after historically relevant styles found in Columbia in the early 20th Century. This approach will foster a historical importance to the pedestrian scale*

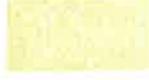
Streetscape Master Plan

Streetscape as Function

There are many perspectives from which a streetscape can be perceived, with aesthetics frequently prevailing over other considerations. This plan looks to the functional aspects of the streetscape and its elements to give meaning and appropriateness to its recommendations and design value.

It is important that the streetscape “works” in regard to dimension, scale, clarity, and context. Elements should serve a purpose and have meaning as opposed to being ornamentation or afterthought. The pedestrian should be able to associate freely to the scale of space, identity, and energy of a streetscape as a result of the improvements and how they function.

Most important to developing functional streetscapes in the urban context is creating appropriate dimensional space and scale. Within downtown street corridors, creating and acknowledging space is critical in that it essentially involves re-creating space, taking from one function and giving to another. For example, streets become narrower so sidewalks can become wider. Parking is removed to create bump-outs for social space. As all of these trade-offs entail compromise, sound reasoning must exist that provides a functional basis for these recommendations and takes these issues into account.



To appropriately evaluate and develop a streetscape Master Plan that pursues a functional basis and appropriateness, the streetscape footprint has been divided into zones, each different and functionally unique, that combine and interact to form a successful streetscape.

The recommendation for improvements set forth in this Master plan are organized by zone, distinguishing dimension, materials, and aesthetic function.

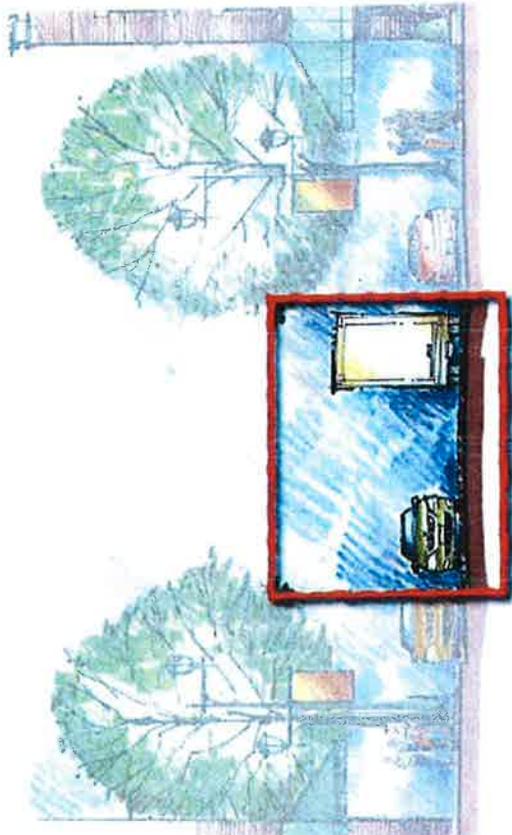


Travelway

The travelway is the section of the streetscape corridor that is the stage for vehicular movement. Its design affects how much traffic can be carried and the speed of travel along the street.

Equally as important, it is where pedestrians cross the street and move from side to side within the streetscape and to connecting uses. Because of its dimension and scale, the expanse of the street can be a barrier to free movement and promote a negative perception of the corridor.

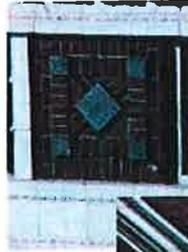
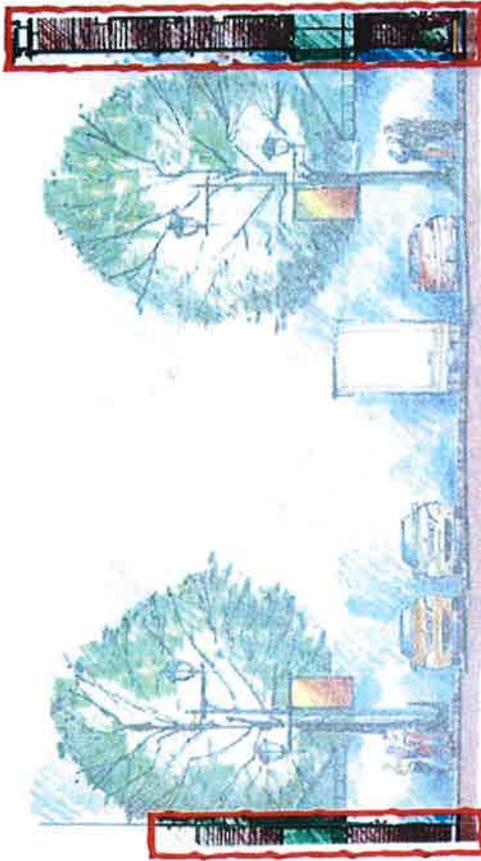
It is important to minimize the visual impact as much as possible by breaking up the lines and edges that define the travelway and creating interest in the paving or adjacent edge treatments. Differential paving treatments at crosswalks and unobstructed sightlines provide clarity and security of pedestrian movement across the travelway. Narrowed lane widths and mid-block crosswalks promote slower traffic speeds and improve the perception of safety.



Development Zone

Defining the edge of the streetscape is the development zone, where land uses meet the street. Elements of the development zone include building façades, open lots, front yards and points of vehicular access. These elements and how they impact the streetscape are shaped by variables such as nature of use, architectural character, façade height, setback, and access.

It is important that the treatment of the development zone, either in dealing with existing buildings through restoration or through redevelopment through infill, be approached through the use of sound urban development standards and guidelines that focus on the character and aesthetics of decisions involving building orientation and placement, streetscape, lighting, landscape themes, and architectural style. All decisions regarding improvements within the development zone should address a pedestrian scale perspective as well as the dynamic interaction of function within the development zone and activity at the street level.





Pedestrian/Amenities Zone

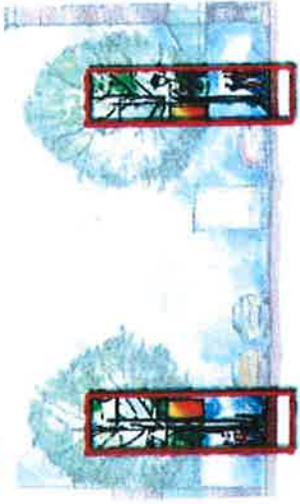
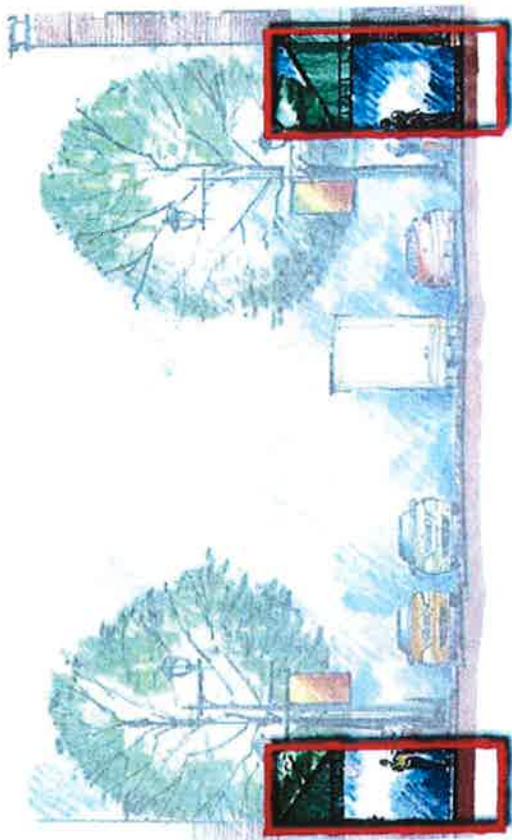
An Important aspect of the creation of safe, active, and livable urban areas is the development of pedestrian friendly streetscapes. Sidewalk activity and vitality encourages interest and participation, key components of economic vigor. A perceived quality of life lends momentum to any redevelopment initiative.

The pedestrian & amenities zone is the section of the streetscape where there is movement of people between land uses, vehicles and within the streetscape framework. Elements of the pedestrian / amenities environment include parking, curb & gutter, sidewalks, landscape, building egress and social space. This zone also serves as the interface between the land use and the travelway, serving to stage movement between the streetscape and the rest of the community.

The organization and character of the elements such as seating areas, sidewalks, landscaping, paving, and seating achieve a dual purpose – they serve to calm traffic speeds and provide an attractive and safer pedestrian area.

In unique areas with special populations such as the Tiger Columns, the elements found in the pedestrian & amenities zones will make a critical difference in the safety, comfort and access of the senior population as they interact with the streetscape.

Within the Avenue of the Columns there exists a great deal of diversity in the nature and origin of pedestrian activity and movement. The character of the pedestrian zone in material, dimension, and function will serve as a unifying element throughout the corridor, giving identity and meaning to the Avenue of the Columns.



General Design Concepts

Design recommendations for the Avenue of the Columns are focused to the concept of creating meaningful pedestrian scale space, acknowledging functional keypoints and focus areas, and creating space for pedestrian movement and interaction.

The typical streetscape dimensional footprint exists within a 65' to 67.5' right of way between buildings. This involves reducing the travelway dimension to 11' to 12' lane widths (22'-24'). The 8.5' to 9' parking section within the pedestrian zone serves to provide parking in front of retail functions or is removed to provide dimensional space for sidewalk widening at key functional nodes along the corridor.

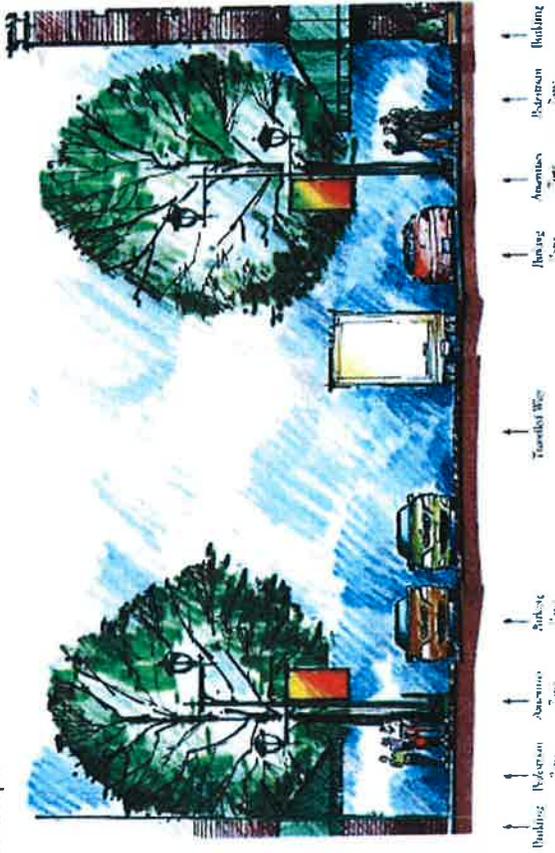
Serving as a buffer between the curb line at the travelway or parking and the sidewalk is the 5' amenities zone. Keyed by pavers of differing colors, this aspect of the pedestrian zone is the location for street trees and grates, raised planter / seatwalls, benches and street furnishings, pedestrian & street lighting, signage, and other streetscape elements that serve the pedestrian function.

Where parking is removed, the amenity zone is expanded to create dimension social space acknowledging key focus points along the corridor including prominent buildings, intersections, and functional gathering spaces.

The interface between the streetscape and the development zone, the sidewalk (6'-8' width)

serves pedestrian movement and linkages throughout the corridor. Additionally, the sidewalk is the front stoop or threshold for the buildings along the streetscape, serving to link the pedestrian to the development zone through window transparencies and doorway entries.

Streetscape interaction and pedestrian movement at the intersections is acknowledged by moving parking away from the intersection, creating dimension in "bump-outs" for social space and raised seatwall planters in the amenities zone. This serves to focus social interaction and gathering while providing shade and structure in the streetscape.



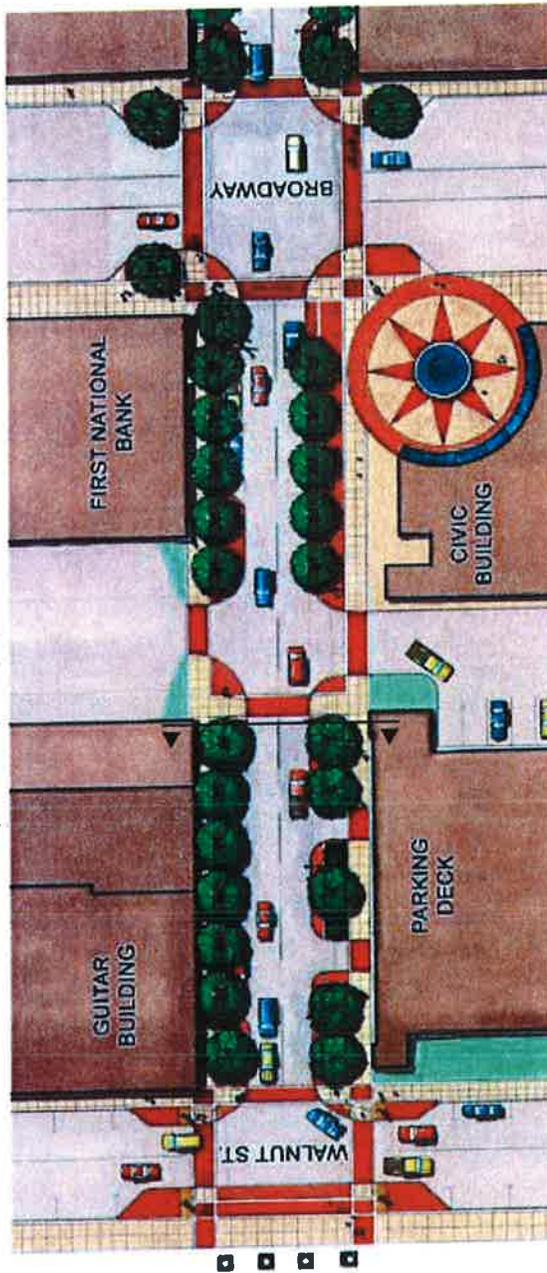


Walnut – Broadway Block

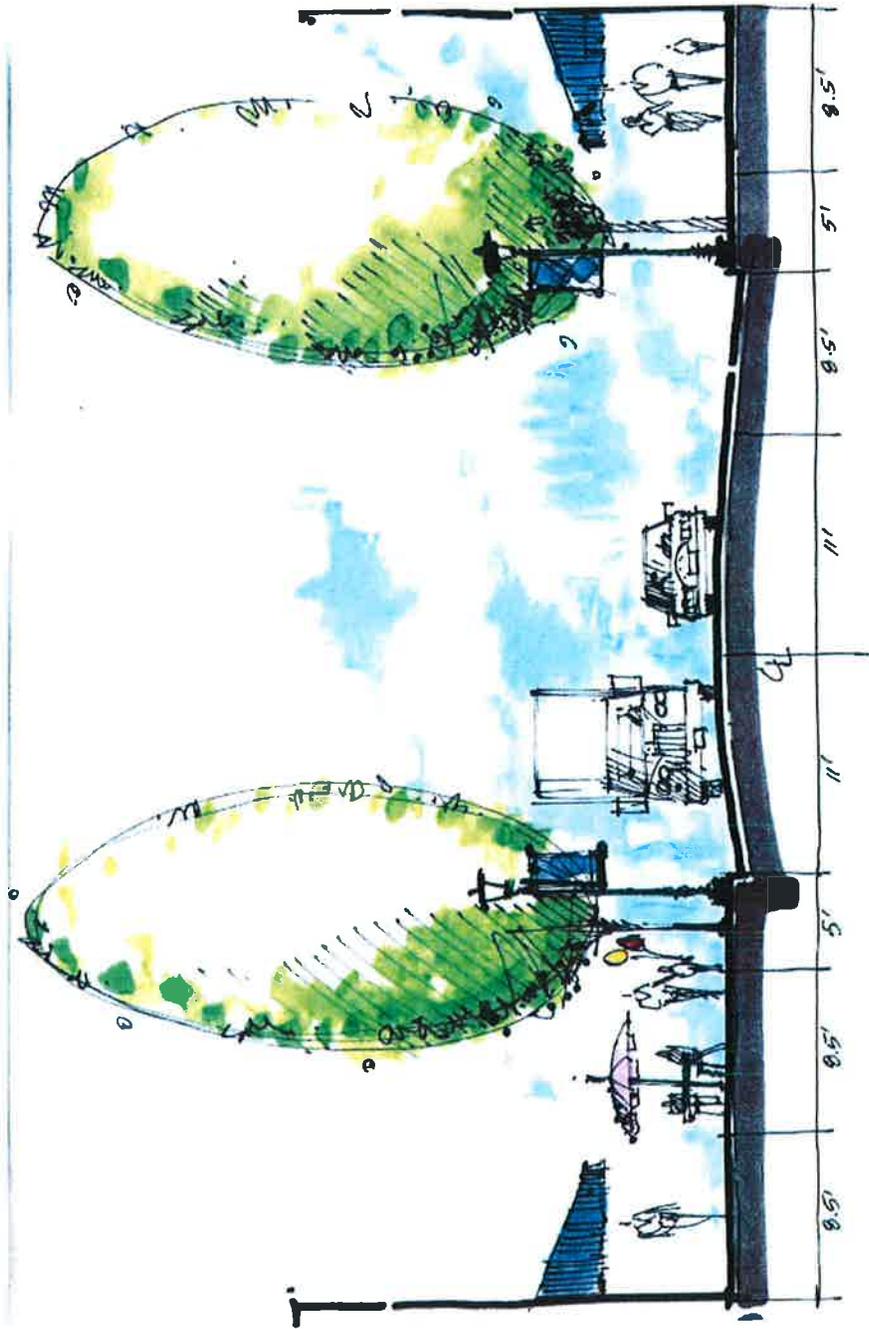
Recommendations for the Walnut–Broadway block respond to the predominately civic and institutional nature of the uses along the corridor. Beginning at the Walnut Street intersection the streetscape treatment emphasizes the pedestrian staging and interaction that occurs at the Courthouse Columns with a widened foreground at the north side of Walnut. Moving south, parking is removed along the west side of 8th acknowledging traffic movements and access dimension at the parking structure and seeking to create social interaction space in front of the new Municipal Building. The proposed plaza in front

of the municipal building will focus interaction at the Broadway Street intersection and keynote activities within this section of the Avenue of the Columns.

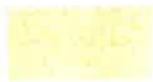
Along the east side of the block, the standard footprint is proposed with on-street parking in front of the mixed use buildings and the bank. Parking is moved away from the intersections and the mid-block parking entrance, creating space for pedestrian movement and interaction. The mid-block treatment also includes a pedestrian crosswalk, focusing pedestrian movement across the corridor in response to the functional cues of parking and destination.



Streetscape Master Plan
Walnut - Broadway Block



Cross Section
Walnut - Broadway Block

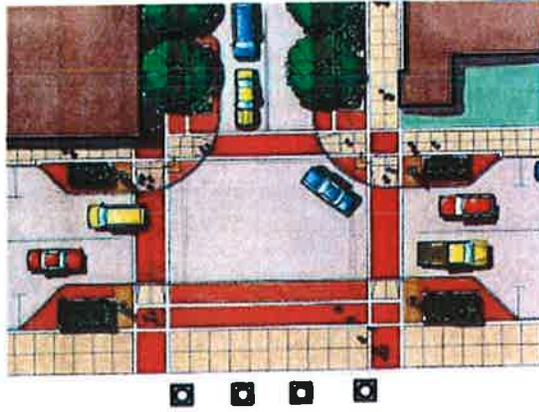


Existing Street Conditions



Courthouse Columns - Proposed Street Image

Streetscape Master Plan
Courthouse Columns Focus Area





Broadway-Cherry Block

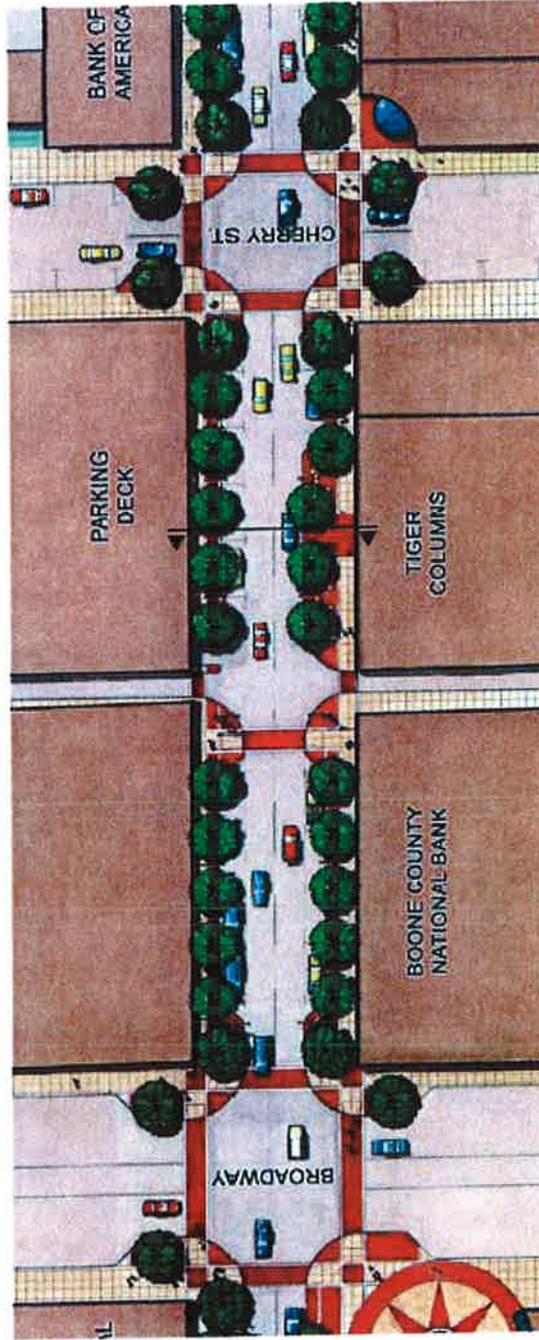
The Streetscape for the Broadway-Cherry Block begins with the focus at the Broadway intersection. By moving parking away from the intersection, space is created for pedestrian scale space that accommodates the high activity level in front of the bank and along Broadway. Raised planters serve to define space and provide shade, while helping to transition the slope as 8th Street moves south.

The typical streetscape treatment is recommended for this block as parking remains on both sides of the street to mid-block where a bump-out and

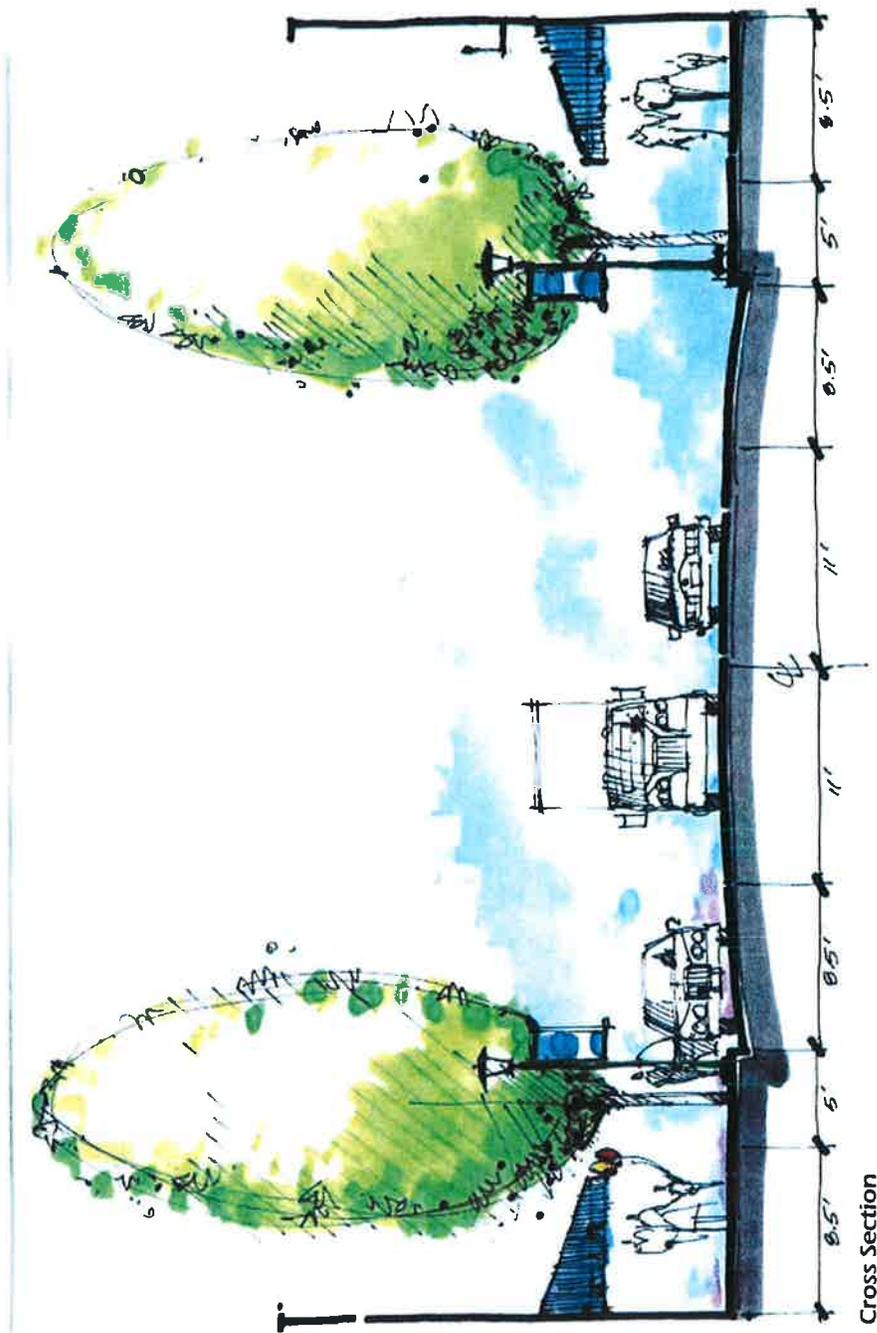
crosswalk addresses pedestrian movement across the corridor and provides a stage for the public art and lower level entry to the bank.

Moving south from mid-block, the parking is removed in front of the Tiger Columns, creating space for social gathering and seating for residents and customers, with limited accessible parking remaining south of the building entrance and in front of the mixed use building at the corner.

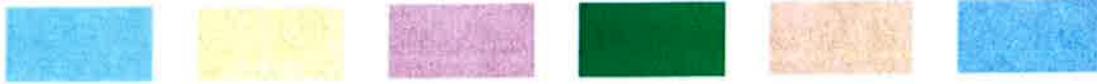
Parking remains on the east side of the block to the intersection with the standard streetscape footprint recommended here.



Streetscape Master Plan
Broadway - Cherry Block



Cross Section
Broadway-Cherry Block

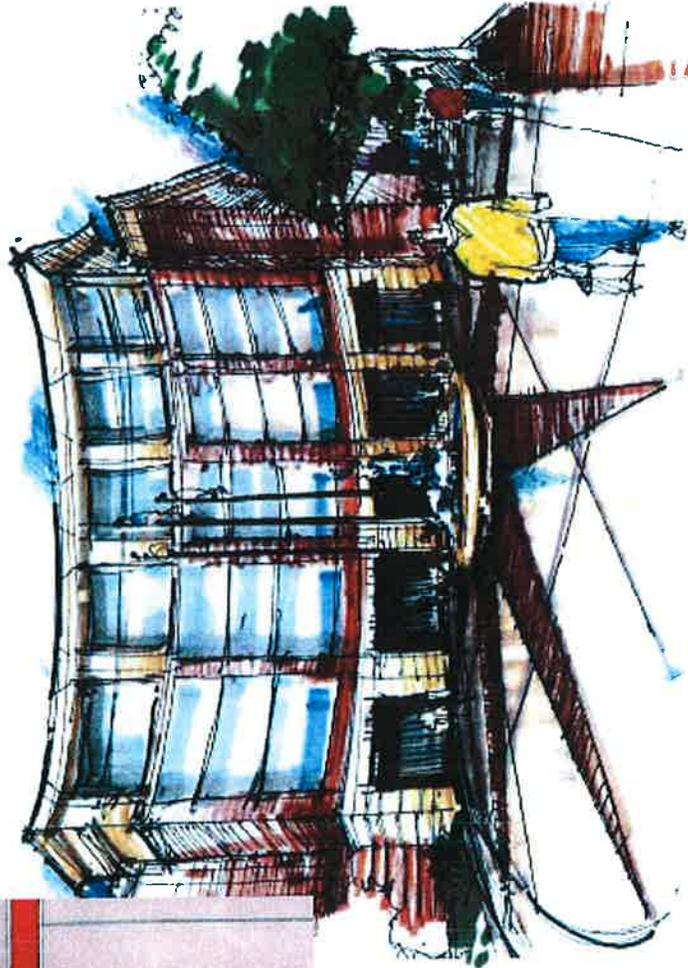
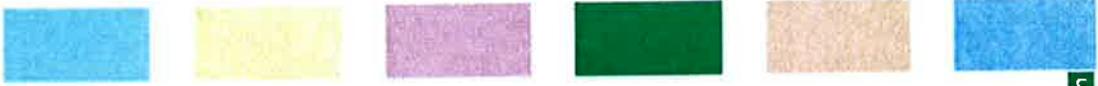
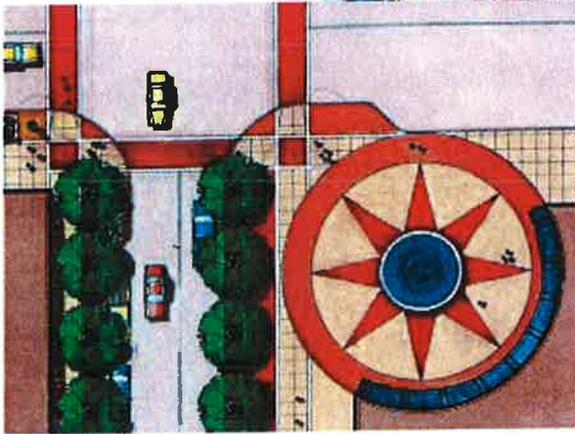


Existing Street Conditions



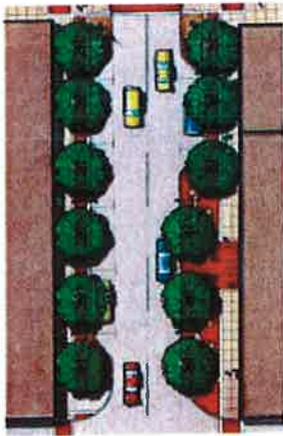
Historic Mixed Use Proposed Street Image

Streetscape Master Plan
Civic Building Plaza Focus Area





Streetscape Master Plan
Tiger Columns Plaza Focus Area



Streetscape Master Plan

Cherry-Locust Block

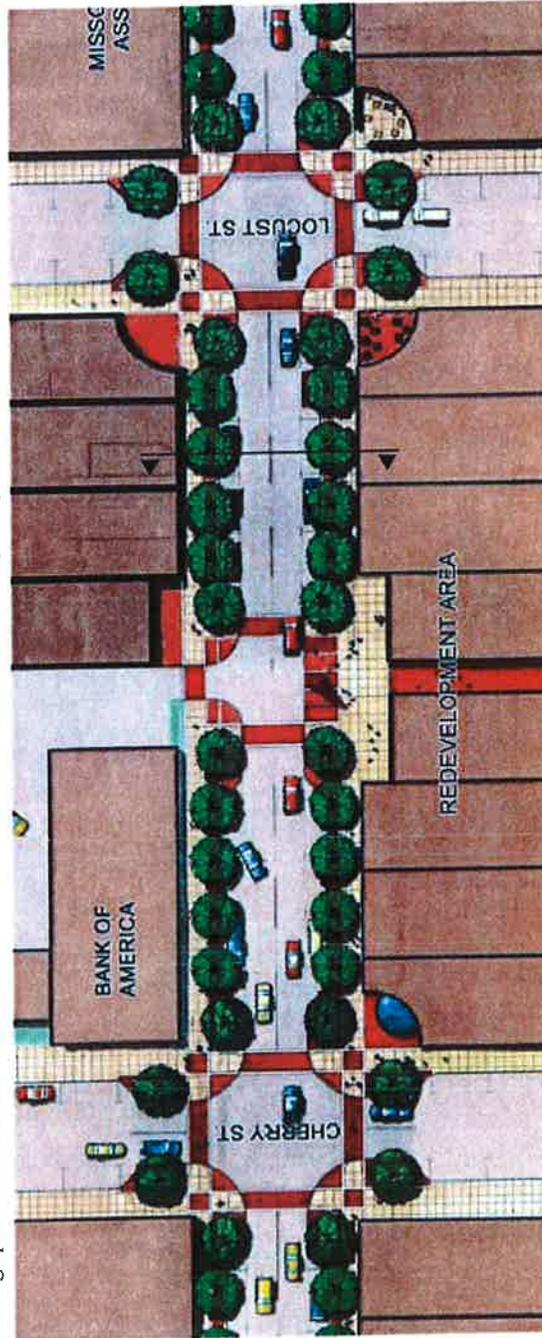
The Cherry-Locust block contains the primary area of redevelopment potential, particularly through the west side of the corridor. The standard streetscape footprint is recommended along the entire block, acknowledging the existing retail and transitional uses along the east side of the street and the redevelopment potential along the west side.

By pulling the parking back from the intersections, widened pedestrian areas allowing for more dimension in the pedestrian and amenities zones, creating space for social interaction and raised

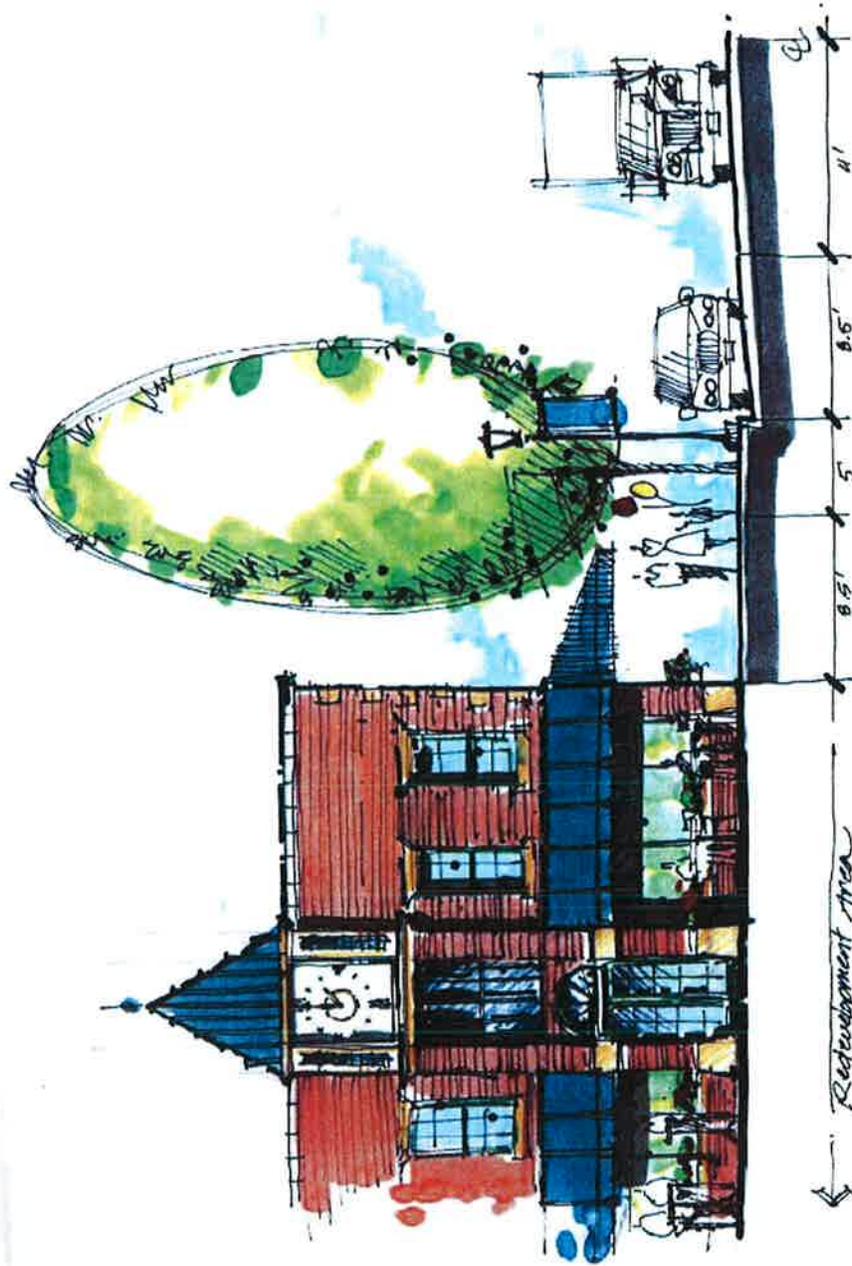
planters. Within the intersection areas along the west side, additional space could be created by angling the entries to future buildings to face the intersections or setting back the corners to create more space and focus.

At the mid-block, a significant plaza or public space should be integrated to focus the entry and cross street interaction. Keyed into this plaza space would be two crosswalks at either side of the alley footprint as it would not extend west through the block. Parking would be removed at the mid-block to create more dimension and focus for the plaza. A significant architectural element such as a clock tower located with this plaza space

would provide for a wayfinding monument or beacon at a midpoint along the Avenue of the Columns, orienting the space and corridor within the crossroads of downtown.



Streetscape Master Plan
Cherry - Locust Block



Cross Section
Cherry-Locust Block

Streetscape Master Plan



Existing Street Conditions

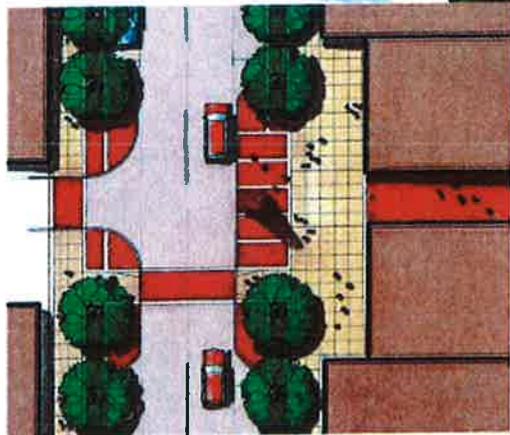


Development Area - Proposed Street Image





Streetscape Master Plan
Redevelopment Plaza Focus Area



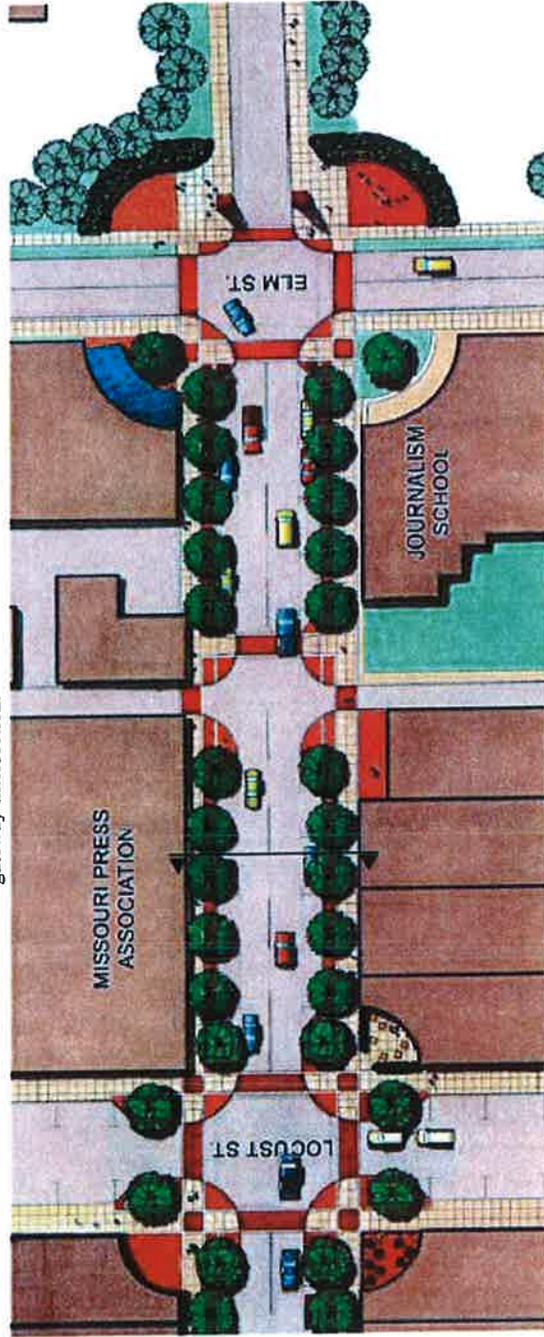
Locust – Elm Block

As the beginning of the transition to the University District, the Locust-Elm Block is keyed to a variety of uses and influences. The opportunity exists on the west side of the corridor for mid-block redevelopment and to create additional mixed-uses. From Mid-block south on the west side and along the entire east side of the street, the uses are predominantly urban campus in nature.

The typical streetscape footprint is recommended along the entire block, with parking pulled away

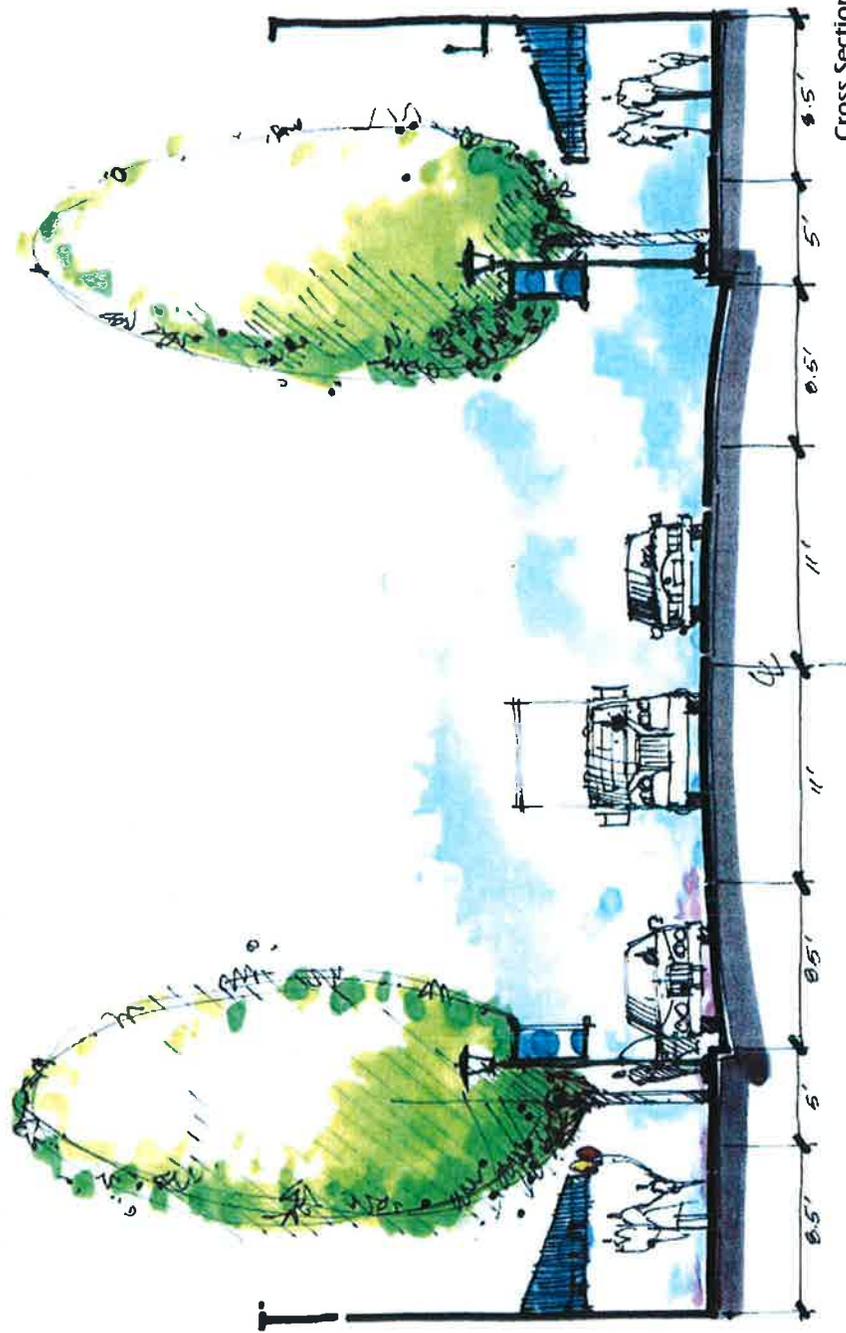
for the intersections and at mid-block. Bump-outs are created at the mid-block with a crosswalk to accommodate pedestrian space and movement across the street to destinations and between the various retail and campus uses.

The intersection at Elm Street is shaped by the orientation and activity of the Journalism School and the gateway to the University on its south side. It is recommended that an architectural feature or monument be placed at the east side of the Elm Street intersection, mirroring the form created at the Journalism School entry and reinforcing the four corner structure of the gateway intersection.



Streetscape Master Plan
Locust - Elm Block

Streetscape Master Plan



Cross Section
Locust-Elm Block

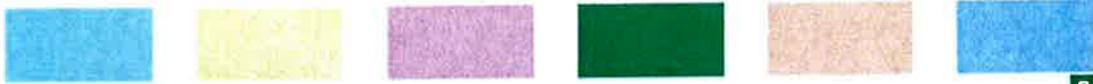
Streetscape Master Plan



Existing Street Conditions

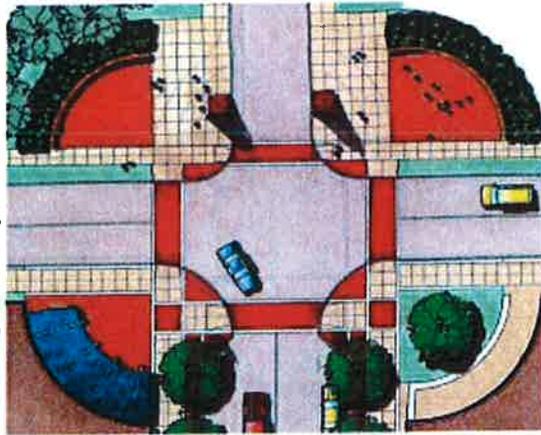


University Gateway - Proposed Street Image





Streetscape Master Plan
University Gateway Focus Area



Streetscape Master Plan

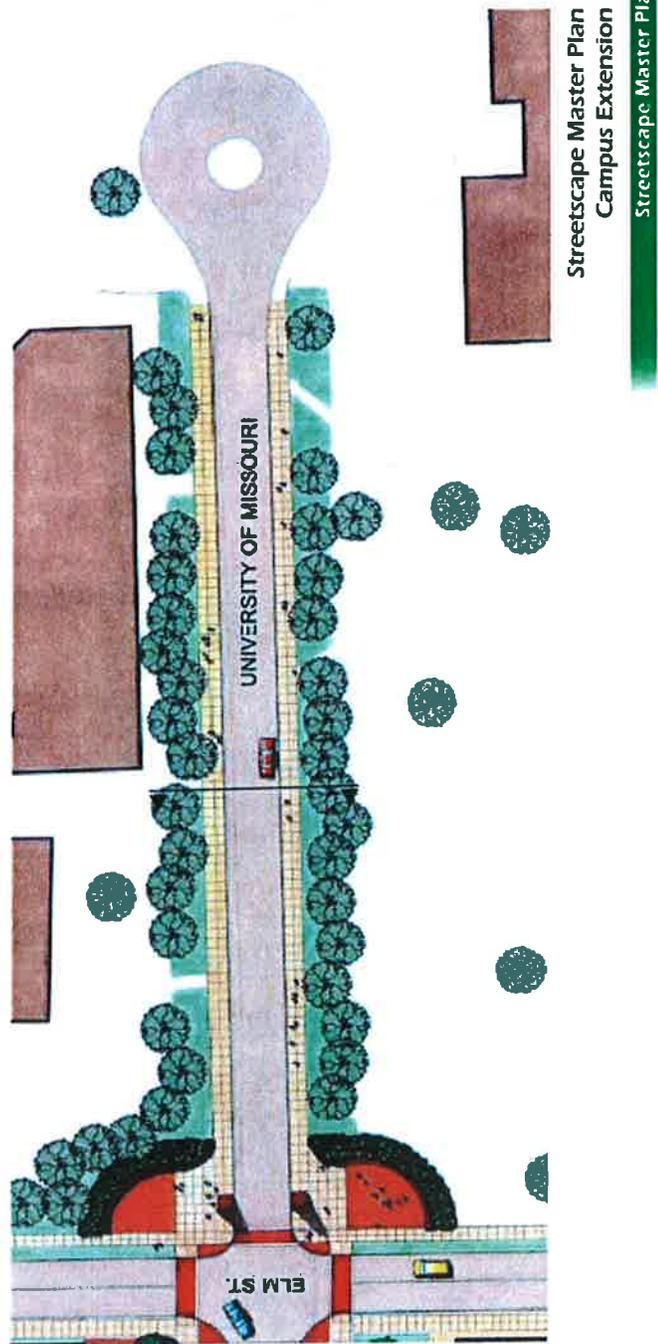


Campus Extension

The Elm Street intersection serves to transition the urban character of the Avenue of the Columns into a more traditional campus setting.

By opening up space around the historic gateway monuments and creating seating and social space within the gateway framework, movement to and through this gateway will be more focused and inviting. Widened sidewalk corridors along the entry drive will promote pedestrian movement and use of the corridor, and the added dimension and lines will provide a greater visual cue and link to

the columns in Francis Quadrangle. These improvements will help to restore the significance of the campus gateway and create a stronger link between the Avenue of the Columns and the University grounds without overstating the extension of the Avenue into the campus framework.





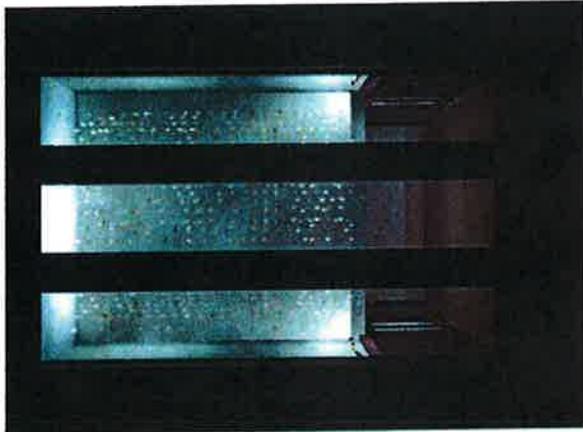
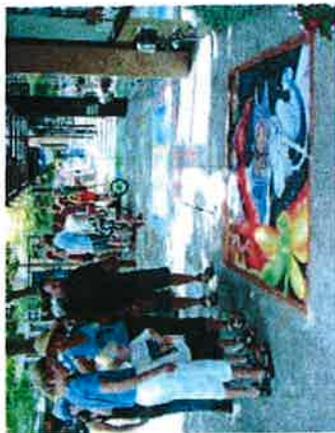
Cross Section
Campus Extension

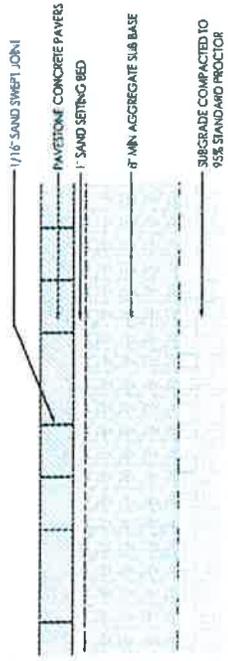
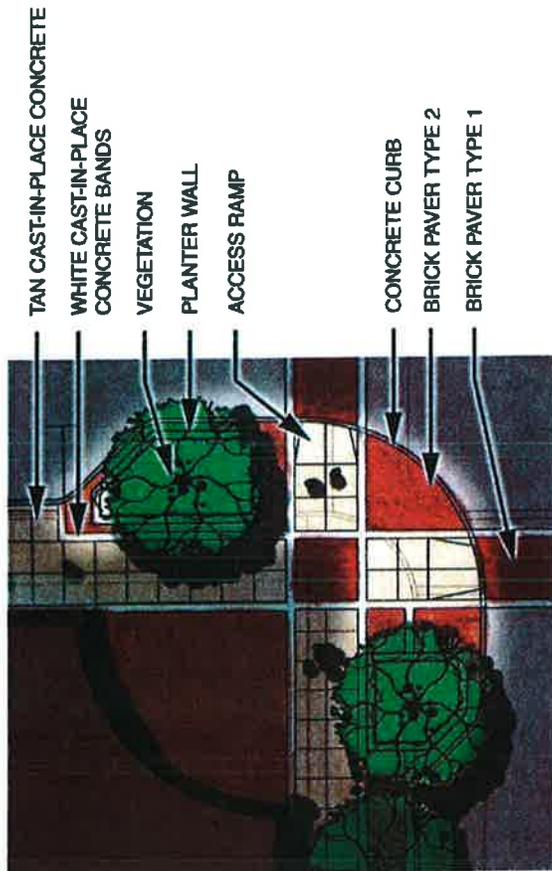
Streetscape Master Plan

Civic Art

The inclusion and focus on artwork or creative elements within the streetscape is a meaningful and important way to create identity and interest along the streetscape. Public art could be programmed along the Avenue of the Columns as an effort to install permanent works or by developing a temporary exhibit initiative. Any initiative to include public art as a component of the public streetscape should involve the city's Standing Committee on Public Art and Commission on Cultural Affairs working with the Office of Cultural Affairs to implement Columbia's already established Percent for Art program.

Of primary consideration to the long term success of any public art installation program or the inclusion of art in aspects of the streetscape is proactive advance planning for management of the collection properties and funding for ongoing maintenance of the installations or venues.





PAVED "BUMP-OUTS"

Bump-outs provide additional pedestrian space at key intersections. These spaces provide a dynamic interface of pedestrian and vehicular traffic ways. Raised seat walls add color and provide seating space in bump-out areas should be incorporated into the bump-out areas.

RECOMMENDATION:

Bump-outs should not interfere with the vehicular pathways along streets and in parallel parking zones.

STYLE:

Bump-outs should be constructed of unit pavers with cast-in place concrete paving bands with coloration.

Unit pavers should also be used at the junction of sidewalks, where crosswalks from a "square".

Bump-outs in the pedestrian zone should be cast in place concrete.

Coordinate material pattern, size and color with paved crosswalks and accessible ramps.

PEDESTRIAN AND STREET LIGHTING

Lighting is a functional requirement that also impacts the visual environment. The lighting system conveys a sense of order and organization, and should be used as a unifying streetscape element.

RECOMMENDATIONS:

Lamp should be metal halide or mercury vapor to promote good color rendition, foliage lighting, life span, and electrical efficiency.

Additional lighting such as bollard lighting, wall-mounted lighting, and spot lighting should be consistent with post lighting.

Pendant Mount with cutoff light to reduce light pollution and visual glare.

Spot lighting and up-lighting should be used to highlight signs, architectural features, and other important objects. Spotlights and up-lights should be screened by landscaping or other methods so they are inconspicuous during the day.

STYLE:

Base of light poles should be customized to a historical standard.

Fluted shaft for sun/shade visual contrast during the day.

Dual cross arms for pedestrian traveled ways and vehicular traveled ways.

Pendant Mount.

Bracket-type banners should be implemented on one side of the lighting. Banner graphics should be coordinated with the City of Columbia.

Coordinate style with signage, bollards and other streetscape amenities.

COLORS:

Cast Iron with Black Powder Coat or Pewter Finish.





SIGNAGE

Signage is used to guide, warn, and identify to users specific streetscape information.

RECOMMENDATION:

Signage should be developed to provide flexibility in order to adapt to changes and additions over time.

Do not place more than one sign at any location in order to reduce visual clutter. Traffic rules are the exception to this rule.

Signage should be simple, readable, and eliminate confusion for users.

Careful consideration should be made for the aesthetic quality of signage on the Avenue.

STYLE:

Signs should be consistent with other materials used in the Avenue of the Columns.

All traffic signage should be in compliance with current state or federal standards.

Coordinate style with pedestrian and street lighting, bollards, and other streetscape amenities.

COLORS:

Cast Iron with Black Powder Coat or Pewter Finish.

BOLLARD

Bollards are utilized to separate vehicular traffic, mark pathways and space, or as decorative elements in pedestrian zones.

RECOMMENDATION:

Use removable bollards where necessary for emergency/service functions.

Use concealed mounting to improve aesthetic appeal

STYLE:

Structural Steel Body with powder coat finish

Cast aluminum or steel top with painted finish.

24" to 32" high, spaced 3' to 5' on center.

Coordinate style with pedestrian and street lighting, signage, and other streetscape amenities.

COLORS:

Black Powder Coat or Pewter Finish.



"Cherry Street" Model from Robinson Iron



"Scarborough" Model Bench by LandscapeForms

BENCH

Benches provide comfort, relaxation, and informal gathering along the entire streetscape.

RECOMMENDATIONS:

Benches should be sited on hardscape surfaces adjacent in the amenities zone.

Provide proper clearance around benches, a minimum of 2'-0" setback from the pedestrian zone.

Place benches under canopy trees for seasonal shade.

STYLE:

Painted Steel with wood or metal seat.

Powdercoat paint finish for durability.

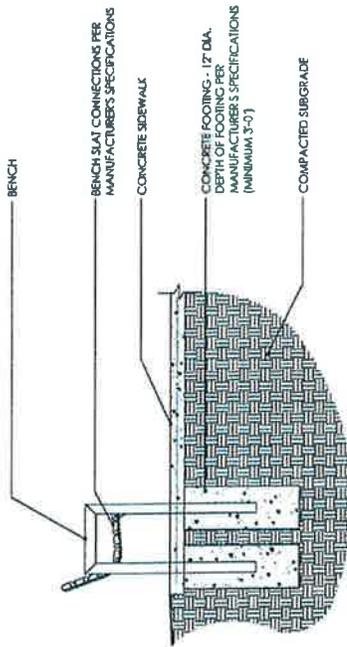
Benches should be at least 5' long to seat 2-3 people comfortably.

Benches should have backs for support, and the city should review if a center arm rest is preferable in order to discourage sleeping.

Coordinate style with trash receptacles, bike racks, vending enclosures and other streetscape amenities.

COLORS:

Black Powder Coat or Pewter Finish.



TRASH RECEPTACLE / RECYCLING

Trash receptacles are necessary to support the informal gathering, resting, eating, and waiting uses characteristic on the streetscape.

RECOMMENDATIONS:

Coordinate size with the City of Columbia to maximize efficiency of collection

Trash receptacles should be located with priority given to corners and outside eating establishments

STYLE:

Painted Steel (powder-coated)

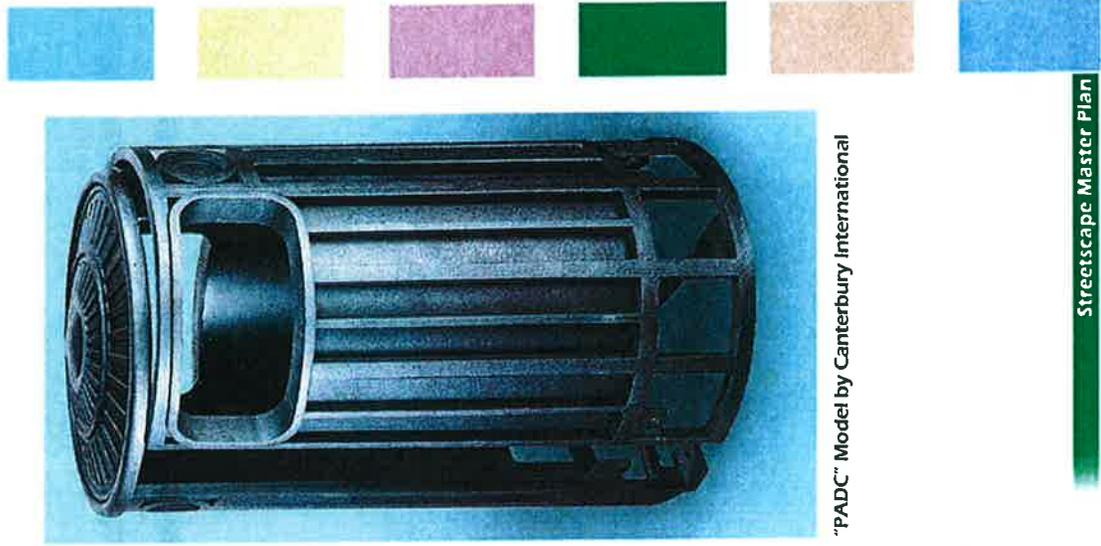
Receptacles should be equipped with polyethylene plastic liners

Hinged side door to ease collection

Coordinate style with benches, bike racks, vending enclosures and other streetscape amenities.

COLORS:

Black Powder Coat or Pewter Finish.



"PADC" Model by Canterbury International



"143-1076" Model by Patterson-Williams

BICYCLE RACKS

Bicycle racks provide another vehicular option for the Avenue of the Columns for users.

RECOMMENDATIONS:

Bicycle racks should be provided at key destination locations.

Bicycle racks should be located on a hardscape surface where they will not impede pedestrian movement or block building entrances.

Embedment mounting for added bicycle security

Racks should accommodate at least 6 bicycles.

STYLE:

Welded construction

Steel Pipe

Powdercoat finish.

Coordinate style with benches, trash receptacles, vending enclosures, and other streetscape amenities.

COLORS:

Black Powder Coat or Pewter Finish.

FENCING AND ENCLOSURES

Fencing

Fenced vending machine enclosures are provided for users to easily access items they need on a daily basis. The location of these machines need to be incorporated into the street currency to provide a unified streetscape.

Enclosures should be located in highly-visible, open locations.

Coordinate style with benches, bike racks, trash receptacles, and other streetscape amenities.

RECOMMENDATION:

Use fenced enclosures to improve the aesthetic arrangement of newspaper vending machines, kiosks, food vending machines, or other utilitarian items.

Structural Steel fence with powder coat painted finish

Fence should be less than 30" high.

STYLE:

Locate vending machine enclosures convenient to high-volume pedestrian areas to attract users and provide security.

Avoid locating vending machine enclosures at building entrances.

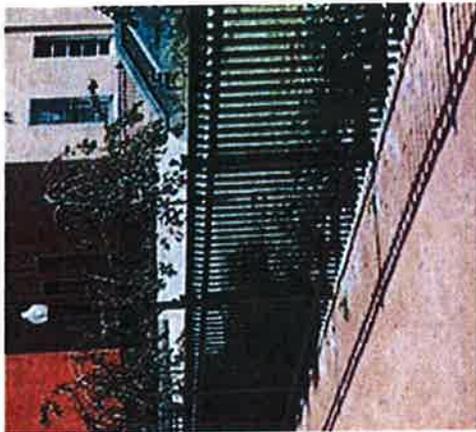
Enclosures should be easily accessible and placed in locations that will not impede pedestrian circulation

Enclosures should be located at least 12" from curb.

Enclosures should be constructed with vandal resistant materials.

COLORS:

Black Powder Coat or Pewter Finish.



"Imperial" Model by Master Haiko Fencing

STREET TREES

Street tree plantings should be used to reinforce vehicular hierarchy, orient and direct traffic, frame views and vistas, and visually de-emphasize parking areas.

RECOMMENDATIONS:

Provide a minimum of 3.5' deep tree pits in amenities zone areas. Also consider the depth of growing space in raised planter areas.

Allow sufficient room for tree canopies to grow and develop without conflict with other building or sidewalk elements.

Install tree grates to protect trees from soil compaction and root damage in pedestrian use areas.

Use a tree with a lighter canopy to allow sun filtration through the tree and enhance natural lighting along the streetscape.

STYLE:

Open canopy tree with a rounded or vase-like form.



Gleditsia triachanthos var. "inermis"



Zelkova serrata "Green Vase"



Ginkgo biloba "fastigiata"



SHRUBS, PERENNIALS, ANNUALS

Shrubs, perennials, and annuals provide added color, texture, and form to the streetscape.

RECOMMENDATIONS:

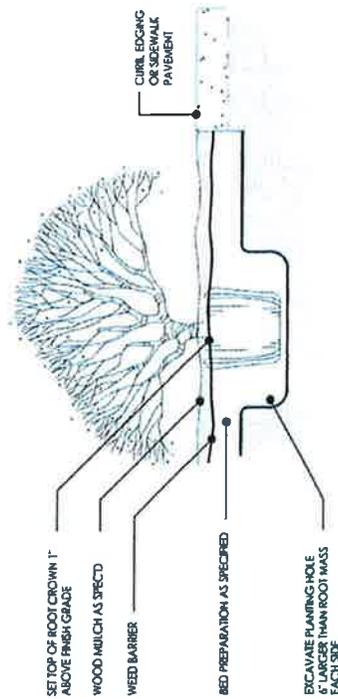
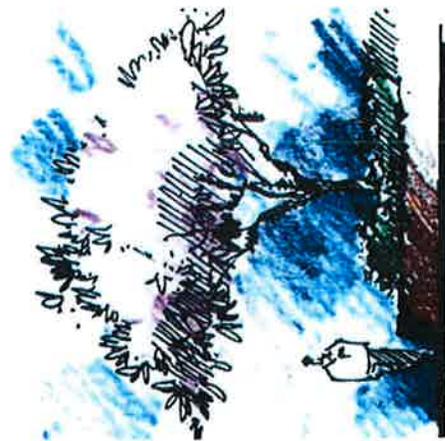
Place plant materials that are appropriately located and sized for the provided spaces.

Plants should be appropriate for the urban condition. Factors to consider include drought tolerance, salt tolerance, and the amount of maintenance required for the plant.

STYLE:

All planting should be complementary and provide contrasting color, texture, and form to improve visual quality.

Planting should create a strong identity for a focal area or district. Consider the use of thematic plantings along the streetscape.



MOVABLE PLANTERS

Planters are used to delineate spaces in the hardscape zones, or as a landscape treatment along a hard building edge.

RECOMMENDATIONS:

- Planters should be movable.
- Planters should contain plant materials during the growing season and should be in good appearance during the winter.
- Plant materials should be well-maintained to improve the appearance of the defined area.
- Planters should be laid out as a border along a building or along a perimeter of defined hardscape space.
- Planters should be a consistent height and width in all border treatments and defined spatial areas.

STYLE:

- Cast or precast stone
- Circular or Squared Box
- 36" tall
- 3'-5' width to allow for clarity and definition of the space

COLORS:

- Color should closely match the stone caps of the raised planter beds, or a complementary warm earhtone.



"Atheneum" Style Planter by Dura Art Stone



"Chinook" Model by Urban Accessories

TREE GRATES

Tree grates provide security for tree root systems, as well as provide additional space for high-use pedestrian paths.

RECOMMENDATIONS:

Tree grates should be used when installing trees in large paved areas.

Design should be pedestrian-safe.

Design should have 16" minimum diameter tree opening size.

Design should be expandable to allow for tree growth.

Grate size should be a minimum 5' x 5' for tree health.

Design should incorporate irrigation and air relief systems for tree health.

STYLE:

Unpainted steel with decorative pattern.

Design should be coordinated along the entire streetscape.

Coordinate style with streetscape amenities.

COLORS:

Black powdercoat or pewter finish.

RAISED SEATWALL PLANTERS

Raised planters are used to separate uses and to frame views established along the streetscape, as well as provide additional seating areas for pedestrian users.

RECOMMENDATIONS:

Raised planters should be a comfortable seat wall height.

Raised planters should not conflict with vehicular sightlines.

STYLE:

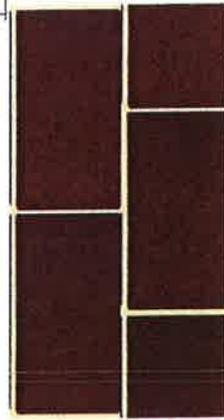
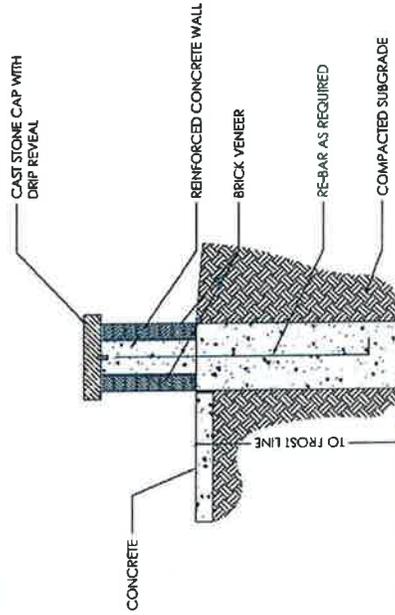
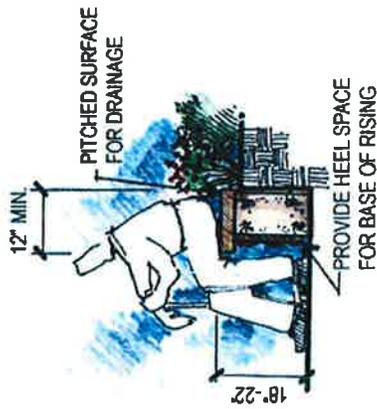
Concrete masonry unit wall with brick veneer finish and stone cap.

Incorporate annuals, perennials, and groundcover in raised planter areas.

Provide weep holes or weep wicks to relieve moisture and maintain plant health.

COLORS:

BMI #30 reddish-brown brick color with light tan cast stone cap.





PAVED CROSSWALK

Paved crosswalks provide a clearly defined pathway for pedestrian users in the traveled way, as well as provide vehicles a cue to slow down or stop at intersections.

RECOMMENDATION:

Crosswalks should be flush with adjacent street paving and sidewalk concrete bands.
Paving should align with accessible curb ramps.

STYLE:

Unit pavers with cast-in place colored concrete bands.

Coordinate material pattern, size and color with accessible ramps and paved bump-outs.

COLORS:

#31 BMI red brick with tan colored concrete banding.

Implementation

Streetscape

The Avenue of the Columns Streetscape vision speaks to the desire for revitalization of an important part of downtown Columbia. This Master Plan document addresses one part of that vision, the aesthetic and functional framework for that vision.

It has been advocated from the beginning of the master planning process that simply addressing the *physical character of the streetscape* will not achieve the vision that guides this effort. Streetscape improvements are functional and aesthetic in nature and are intended to facilitate and serve the needs of the pedestrian component of the corridor. These types of improvements do not *create* or expand that pedestrian component. Reinvigoration of retail, residential, or commercial activity is the surest way to change the nature of street activity and vigor. Changing the nature of use or creating more and broader activity is the only way to fundamentally support a vibrant and healthy Avenue of the Columns. The streetscape serves as the stage on which this increased activity will function.

In the course of this process areas of opportunity for this type of redevelopment and reinvigoration have been revealed and considered. In consideration of these opportunities it has been underscored that there is a need to consider redevelopment on a broader scale, considering the entire downtown as the footprint and looking to

more sweeping tools and approaches. There is a great deal of momentum and activity in the downtown area already, but there remains a great deal of potential.

Because of its prominence and meaning within the community, the Avenue of the Columns project could serve as a model initiative in this broader effort, supporting a larger movement toward downtown revitalization.

Role of the Master Plan

This Master Plan serves as a guide and framework for the development of the streetscape improvements along the Avenue of the Columns. This framework outlines design concepts and strategies for the various physical aspects of the corridor and responds to the functional realities and opportunities that have been identified and considered during the process. It is not the final step in the design and implementation process.

Actual design and documentation of these improvements is beyond the scope of this Master Plan. Consideration and final design of the recommended improvements must be undertaken through subsequent design efforts. It will be an extensive and complex process that will likely require a creative and collaborative approach to funding and implementation.

The guidelines set forth in this plan will serve to direct future streetscape improvements so that the result of phased or incremental efforts will be consistent in their nature and identity and true to the vision of the committee responsible for this plan.



Phasing

Implementation of the improvements represented in this Master Plan can be approached and phased in a multitude of ways. Depending on the funding structure and timeline, the implementation could occur as a single representative project or in single block phases. Implementation of certain segments of the streetscape could occur in conjunction with other keynote projects such as redevelopment infill or the new Municipal building.

It is recommended that broader goals for other improvements such as utility or other infrastructure improvements be considered in conjunction with these projects both in budgeting and staging so that any inherent efficiencies may be realized.

Funding

There is a broad range of funding vehicles and economic tools that are available to projects of this type for implementation. They range from self funding on the municipal level as a capital project to various forms of incentives, abatements, and public/private partnerships. It is recommended that legal advice and counsel be sought as the project moves forward, to direct and structure an approach that best fits the goals and realities of this project. A combination of tools or incentives will most likely be appropriate as it is rare that any single funding mechanism or approach is comprehensive enough to sustain a project of this type.

A summary of funding scenarios or implementation approaches that might be considered for the Avenue of the Columns and associated projects is as follows:

When evaluating an economic development tool for any type of development, or public improvement, the character of the development or improvement being financed guides which economic development tools may be most appropriate. In addition, whether the public or private sector, or both will participate in funding also guides the final determination.

Avenue of the Columns Streetscape

The first category of development for the identification of funding opportunities and approaches is the implementation of the Avenue of the Columns Streetscape Master Plan (“Streetscape Plan”). The recommended funding approaches might involve commitments of the public sector, private sector and/or both.

Community Development Block Grant (CDBG)

CDBG is a federal program which provides entitlement cities, such as Columbia (“City”), with funds to be used to benefit low to moderate income persons, address slums and/or blighted areas or address a particular urgent community need (disaster). When using this funding to benefit an area of the city, the area must be identified as a low to moderate income area as determined by the census tracts. According to the City’s website mapping, the Streetscape Plan area is located in a CDBG Eligibility Area. Streetscape is a public improvement that can be financed through this program. Funding is determined through an annual application and public hearing process with the final decision to be made by the Mayor and City Council. An application may be submitted by a company, organization, person or the City itself. The City currently receives approximately \$1,000,000 annually for this program. Because of the limited funds available for distribution in the entitlement program in contrast to the demand, this financing method

should be considered a phased implementation funding alternative.

Neighborhood Improvement District (NID)

Neighborhood Improvement Districts are established by an election within a defined area called a district or by petition of 2/3rds of the owners of property in the proposed district. In either approach, the final decision to establish the district is made by the Mayor and City Council. This approach allows the Mayor and City Council to make improvements that confer a benefit upon property within the proposed improvement district, by issuing temporary notes or general obligation bonds for the purpose of making the improvements and retiring the indebtedness by special assessment of the property benefited. Benefited properties are not necessarily immediately adjacent to the improvement. The assessment may be made on the basis of area, lineal measure, or any other reasonable method and may not exceed 20 years. The definition of “improvement” in the NID statutes, includes, but is not limited to, landscaping, lighting, streets, crosswalks and the ability to acquire and improve any other public facilities or improvements deemed necessary by the governing body of the city.

Community Improvement District (CID)

Community Improvement Districts are established by a petition of (a) more than 50% of the owners by assessed value of real property located within the proposed district and (b) more

than 50% per capita of the owners of real property within the proposed district. The final decision to establish the district is made by the Mayor and City Council. The district may be a not for profit or a political subdivision. If the district is a political subdivision the cost of improvements or services may be financed by special assessments, real property taxes, business licenses and/or sales tax. If the district is a not for profit the cost of improvements or services may only be financed by special assessments. The governing form of the district will depend upon the magnitude of cost of the Streetscape Plan. If this financing alternative were used to implement the Streetscape Plan only, the not for profit formation may be more appropriate. However, if the financing might include additional costs related to contracting with a private property owner for demolition and removal, renovation, reconstruction or rehabilitation of any building or structure owned by such private party, or parking garages, additional sources of tax revenue may be required. A streetscape, demolition and renovation, reconstruction or rehabilitation, and parking facilities are all specific project costs that can be paid for through a CID.

Corridor Redevelopment Projects

A second category of development for the identification of funding opportunities and approaches is the redevelopment of the "soft" parcel(s) along the Avenue of the Columns. The pilot projects identified for potential redevelopment involve the acquisition of property, demolition of existing structures and improvements and the construction of a mixed use (residential and commercial) development with parking, and potential institutional use development. Institutional uses pose a challenging financing circumstance because any government owned facility will be exempt from real property taxes which are a critical piece to most funding methods for urban redevelopment. For purpose of this analysis the assumption is any institutional uses will be subject to ad valorem taxes.

The recommended economic development incentives for the downtown redevelopment projects are Chapter 353 Urban Redevelopment Law ("Chapter 353") or Tax Increment Financing ("TIF"). The common characteristics of these tools are the ability to utilize eminent domain and the use of revenues from increases in assessed valuation.

Chapter 353

Chapter 353 provides for tax abatement on the improvement (building and fixtures) constructed as part of the redevelopment. The developer receives a tax liability savings which is presumed to offset the extraordinary costs of

redevelopment. The determination as to the appropriateness of this tool will be in whether the tax abatement will adequately provide the necessary financial support for the project related to the developer's increased development costs to make the project financially feasible. The City may also have the option of contributing towards demolition costs from general fund revenues, including the city general sales tax. In addition, low income tax credits and historical tax credits could be applicable depending on the project. The tax credit program is often used in conjunction with Chapter 353 for urban residential redevelopment.

Tax Increment Financing

TIF provides redevelopment project cost financing from the taxes paid attributable to the increase in assessed valuation as a result of the project and 50% of the increase in economic activity taxes (primarily sales taxes) as a result of the project. Depending upon the City's discretion in applying these funds, TIF could provide financing for acquisition costs, demolition, a privately owned parking facility, public improvements and professional service fees related to the development. TIF could generate a greater amount of financing for the project than Chapter 353.

Comprehensive Redevelopment Downtown

A broader category of development for identification of funding opportunities and approaches is for a comprehensive downtown redevelopment initiative. The potential redevelopment area would be comprised of 10th or 11th Street on the east to Providence on the west, and from the University of Missouri campus (Elm Street) on the south to the Courthouse Square (Walnut Street) on the north (“Redevelopment Area”). A project of this scope is difficult to propose definitive public financing options for without a comprehensive land use and market analysis.

The first step in determining financing alternatives would be the establishment of a community leadership vehicle or the reinvention of an existing vehicle such as the LCRA (Land Clearance Redevelopment Authority) that would insulate the governing body and be viewed as an objective vehicle for process and decision making. Next would be to frame a Downtown Redevelopment approach or plan, inclusive of a market analysis, setting forth in general terms what the citizens, developers, existing businesses and policy makers believe are the priorities and the best use of the land within the proposed Redevelopment Area. A Downtown Redevelopment Plan, itself, provides a guide to the development community as to what the City believes is appropriate development in the area. This plan provides a more predictable development environment. With an aspect of risk in the City approval process mitigated, such plans can foster development independent of incentives.

The planning process can bring together information from supporting studies such as the recently completed housing study, and neighborhood development initiatives such as the Avenue of the Columns Master Plan or the Flat Branch Development Plan. Also, other existing policies and ordinances of the City that are applicable to the development of the proposed Redevelopment Area will be fundamental to this approach. Such a plan would serve as the required documentation basis of a Land Clearance Redevelopment Authority (“LCRA”), Planned Industrial Expansion Authority (“PIEA”), or Downtown Economic Stimulus Authority under the Missouri Downtown and Rural Economic Stimulus Act (“MODESA”).

A Downtown Redevelopment Plan and the market place’s reaction to the initial and second redevelopment projects mentioned above will assist in identifying the appropriate financing or funding program(s) for the redevelopment of the Redevelopment Area. We could then clearly identify and recommend the most desirable development incentive approach for implementation of such plan and assist in the establishment of (or reestablishment in the case of the LCRA) the authority

All three alternatives, LCRA, PIEA and MODESA provide varied funding opportunities. LCRA and PIEA involve the use of tax abatements similar to the Chapter 353 program. MODESA utilizes revenue from the same sources as TIF with the addition of potential state income tax or state general sales tax funding. All three programs have governing bodies separate from the City, but still require varying degrees of approval of the City regarding the final financing plan.







PRE-PROPOSAL CONFERENCE SIGN IN SHEET

19-01MAY12 – Landscaping Services for Courthouse Plaza Renovation

Thursday, April 5, 2012, 10:00 a.m. central time

	Representative Name	Business Name	Telephone Number	Fax Number
1.	Melinda Bobbitt	Boone County Purchasing	886-4391	886-4390
2.	Bob Davidson	Boone County Facilities Maintenance	886-4401	
3.	Matt Vander Tuig	Bartlett & West	573-659-6713	573-634-7404
4.	JAKE FRINK	ROST INC	573-446-4405	573-446-0899
5.	TIM H. MALONEY	ROST INC	"	"
6.	ANDREW HERNON	ROST, INC.	"	"
7.	COOY DARR	A CIVIL GROUP	817-5750	817- 5750 1677
8.	PHILIP DIERKER	DESIGNER- LANDSCAPE	443-6010	
9.	Laura Rogers	LANDesign	636-332-4500	636-639-8757
10.	Joan Harding	Stone Creek Landscape	572-474-1215	573-474-1215
11.	Pat Banninger	Boone County Collector	886-4285	886-4294
12.	Jane Pateford	Boone County Auditor	573.886.4278	573.886.4280
13.	Gene Miller	Boone Co. Commission	573-864-2400	
14.	GENE HARDING	DESIGNER LANDSCAPE	573-446-6010	573-446-3888
15.				
16.				
17.				